



Post Falls Highway District Transportation Plan

September 2018



Prepared by:



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ENGINEERS ♦ PLANNERS ♦ SURVEYORS

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Introduction

This Transportation Plan (Plan) has been developed for the Post Falls Highway District (PFHD or the District) in accordance to the general guidelines provided in the Local Highway Technical Assistance Council's (LHTAC) guidance document *Manual on Transportation Plans*.

Purpose

The purpose of this Transportation Plan is to provide a guideline for the District when planning for current or future roadway maintenance or construction, while incorporating regional transportation planning efforts of neighboring jurisdictions and the Kootenai Metropolitan Planning Organization (KMPO), the vetted goals of the District, while also incorporating the public feedback of its citizens.

While this plan has specific projects identified in the CIP, the District still has the flexibility to incorporate other projects based on need, priority, and the availability of funding opportunities.

The components of the PFHD Transportation Plan includes:

Public Involvement

A Public Involvement Plan, which began with stakeholder questionnaires to gather input on issues of importance to our stakeholders. This was followed with the development of a Technical Advisory Committee (TAC) comprised of transportation leaders within our community, who provided insight and direction as to areas of concern and lessons learned from their experiences. To ensure the Public Involvement Plan was well represented, the District had two Public Open Houses to gather input from the public as to concerns and areas to be considered when planning improvements and to provide comments concerning the developed draft capital improvement plan.

Land Use and Growth Impacts

An analysis of existing and proposed land use and growth impacts was performed to identify areas experiencing and expected to experience growth both residentially and commercially. By understanding these trends within the District, decisions can be made proactively, rather than reactively, to plan and prepare for the demands that growth brings to our roadway system.

Existing Conditions and Future Plans

The Transportation Plan included a gathering and analysis of published Transportation Plans from jurisdictional neighbors, a review of the roadway network functional classification, an examination of existing safety concerns, and an inventory of the inter-modal transportation facilities in the District. The PFHD Road Network Map was updated based on the compilation of data gathered during the development of the Transportation Plan.

Pavement Management, Bridge, & Sign Inventory

A review of the current District's Pavement Management Plan in IWorq was performed with recommendations on a system of planning integral with the CIP plan. The review identified the successes of the current plan and how the IWorq program provided a metric by which to plan and develop the capital improvement plan. Through this effort, areas in which the use of the IWorq program could be utilized to benefit the District more fully were identified. The IWorq program includes an inventory of the District's roads, culverts, and signs.

Capital Improvement Plan

A Capital Improvement Plan was developed through the analysis of all of the above, with emphasis on safety, areas of growth, coordination with improvements planned in neighboring jurisdictions, and input from the public open houses.

Implementation Plan

An Implementation Plan was developed to provide a framework to assist the District in annual updating of the CIP, budgeting costs for maintenance and construction, and direction for funding opportunities.

Transportation Plan Funding

The District received funding for this Transportation Plan through the Local Rural Highway Investment Program (LHRIP) administered through LHTAC. LHTAC supports Transportation Plans for Local Public Agencies as they provide an opportunity to effectively plan transportation infrastructure improvements with the collaboration of neighboring jurisdictions, the Technical Advisory Committee, the Public, and local transportation organizations such as the Kootenai Metropolitan Planning Organization.

Post Falls Highway District Background

The Post Falls Highway District was formed in March of 1971, formerly called the Pleasant View Highway District. After the State of Idaho passed a law to consolidate numerous smaller roadway districts into four districts within Kootenai County, Post Falls Highway District, Lakes Highway District, East Side Highway District, and Worley Highway District formed the Associated Highway Districts of Kootenai County, to create a collaborative union for planning and funding a collective effort in maintaining and growing the local roadway system.

The PFHD maintains 191 miles of roads, with 555 culverts, 2 bridges and over 2,500 signs. The PFHD is led by three elected Commissioners, a Road Supervisor, Clerk, office staff, and a multitude of road crews.

There are 7 cities located within the PFHD that share the District's property tax base, with a 2010 US Census population of 54,224. The current yearly budget falls just under \$8 million to maintain and build the roads within the PFHD roadways system. The jurisdiction of the Post Falls Highway District within the Associated Highway District is shown in Figure 1. The Post Falls Highway District Map is shown in Figure 2.

ASSOCIATED HIGHWAY DISTRICTS BOUNDARIES KOOTENAI COUNTY, IDAHO

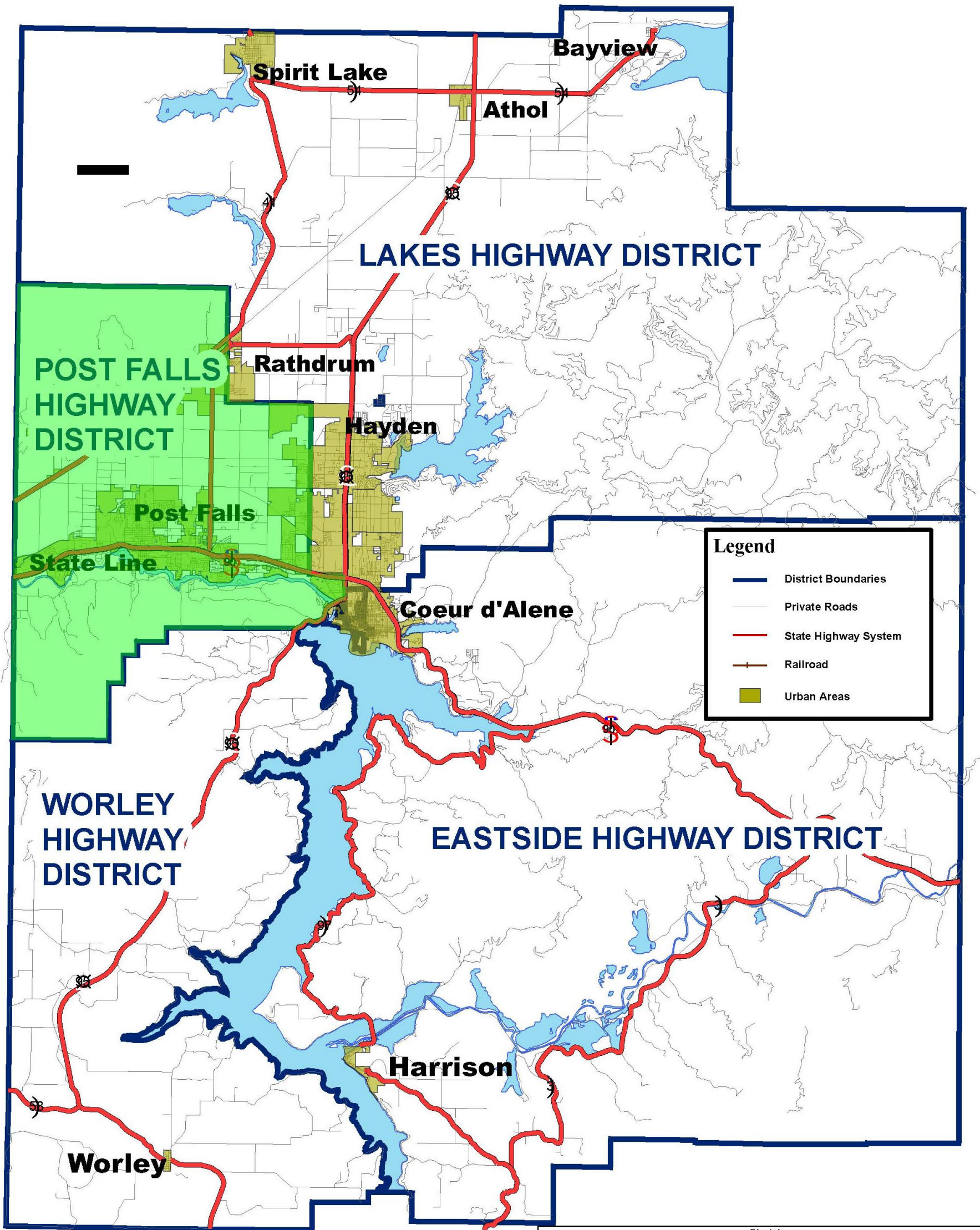


Figure 1 – Associated Highway Districts of Kootenai County



POST FALLS HIGHWAY DISTRICT

ROAD NETWORK MAP

KOOTENAI COUNTY, IDAHO

2018

LEGEND

- HIGHWAY DISTRICT BOUNDARY
- SECTION LINE
- RAILROAD TRACKS
- INTERSTATE HIGHWAY
- U.S. HIGHWAY
- STATE HIGHWAY
- PAVED ROAD
- GRAVEL ROAD
- UNMAINTAINED ROAD
- DEDICATED RIGHT-OF-WAY
- DEDICATED RIGHT-OF-WAY WITH EXISTING TRAVELWAY
- POST FALLS HIGHWAY DISTRICT OFFICE

SUBDIVISION INDEX

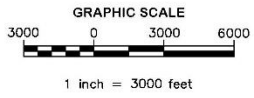
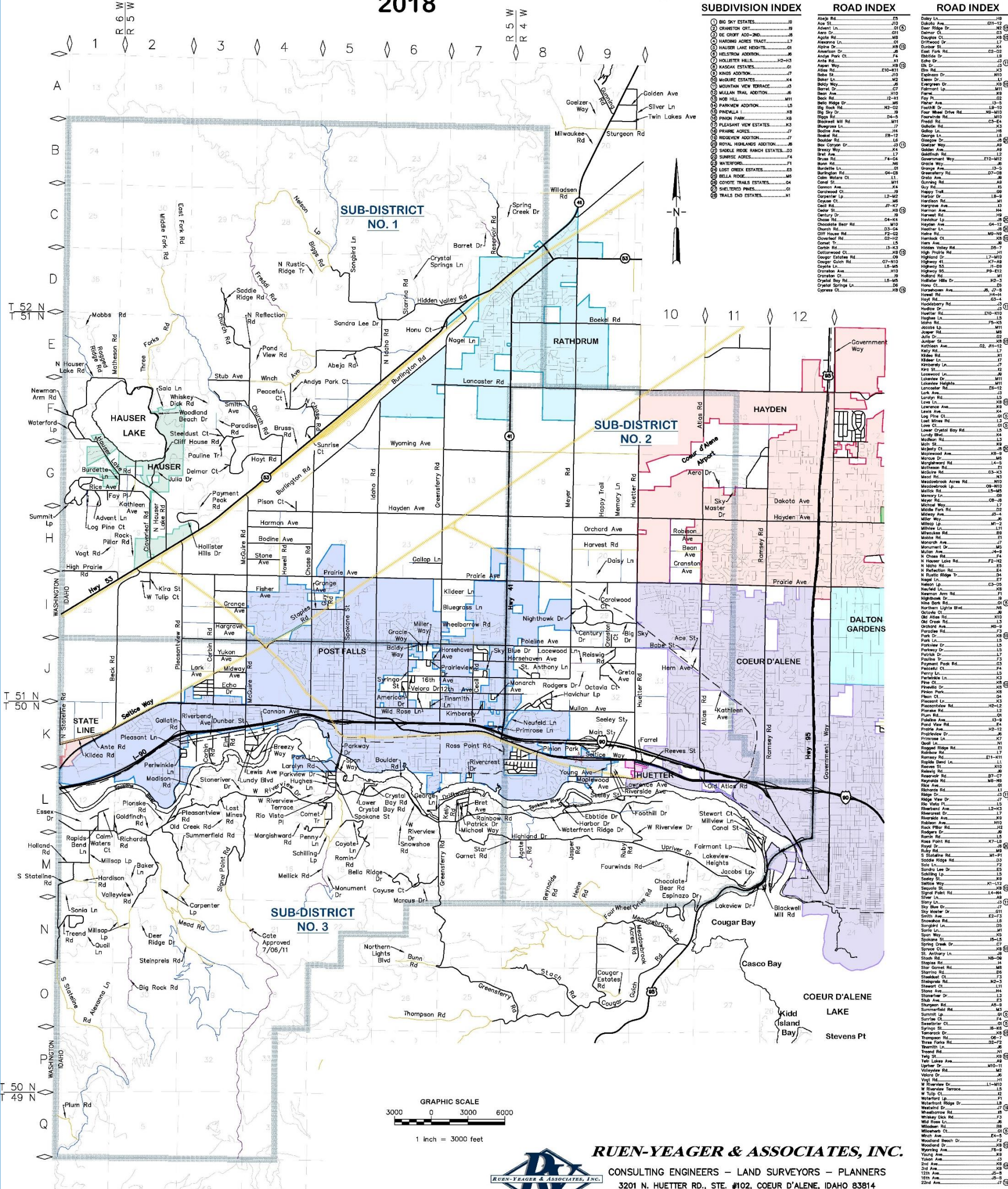
- 1 BIG DRY ESTATES.....18
- 2 CHAMBERLAIN CRT.....19
- 3 DE CROFT ADD.-IND.....18
- 4 HARBINGERS TRACT.....17
- 5 HAUSER LAKE HEIGHTS.....01
- 6 HELSTROM ADDITION.....16
- 7 HOLLISTER HILLS.....10-13
- 8 KASCAK ESTATES.....01
- 9 KINGS ADDITION.....17
- 10 KINGSLEY ESTATES.....14
- 11 MOUNTAIN VIEW TERRACE.....13
- 12 ROSE HILL.....11
- 13 PARKVIEW ADDITION.....15
- 14 PINEVILLE.....18
- 15 PINNACLES.....18
- 16 PLEASANT NEW ESTATES.....13
- 17 PRIMAIRE ACRES.....17
- 18 RIDGEVIEW ADDITION.....17
- 19 ROYAL HIGHLANDS ADDITION.....16
- 20 SABLE ROSE RANCH ESTATES.....13
- 21 SHILOH ADDITION.....17
- 22 SUNRISE ACRES.....14
- 23 WATERFORD.....17
- 24 WEST OAK ESTATES.....13
- 25 BELLA ROSE.....14
- 26 COYOTE TRAILS ESTATES.....04
- 27 SHELTERED PINES.....01
- 28 TRAILS END ESTATES.....11

ROAD INDEX

- 05 Baby Ln.....05
- 06 Aox St.....01
- 07 Deer Ridge Dr.....10
- 08 Dalmor Ct.....03
- 09 Douglas Ct.....13
- 10 Driscoll Dr.....17
- 11 Durbin St.....04
- 12 East Fork Rd.....02-02
- 13 Ebbtide Dr.....11
- 14 Eden Dr.....11
- 15 Elk Dr.....11
- 16 Elm St.....11
- 17 Express Dr.....11
- 18 Evergreen Dr.....10
- 19 Fairport Ln.....11
- 20 Farm.....03
- 21 Fay Pl.....11
- 22 Fisher Ave.....13-15
- 23 Four Wheel Drive Rd.....10-11
- 24 Founders Rd.....11
- 25 Fossil Rd.....04
- 26 Galatin Rd.....13
- 27 Galatin Rd.....18
- 28 George Ln.....16
- 29 George Dr.....16
- 30 Goezler Way.....10
- 31 Golden Ave.....10
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- 33 Govanney Way.....11
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ROAD INDEX

- 101 Baby Ln.....05
- 102 Dakota Ave.....01-12
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Figure 2 - Post Falls Highway District Map

Public Involvement

Public involvement was the first component tasked in developing the Transportation Plan. The Post Falls Highway District reached out to the public through multiple venues to gather input regarding the public's areas of concern, to share the Highway District's knowledge of concerns and needs, and to strengthen public support in the development of the Transportation Plan. The Post Falls Highway District Commissioners and Road Supervisor participated in all public involvement events, making themselves available for questions and providing answers specific to their district. The Public Involvement effort included Stakeholder Questionnaires, three Technical Advisory Committee meetings, and two Public Open Houses. The sequence of Public Involvement efforts is summarized below:

Stakeholder Questionnaires

Stakeholder Questionnaires were sent out in March of 2017 to a list of key stakeholders identified by the Post Falls Highway District Commissioners and the Road Supervisor. Stakeholders were chosen based on their specific inter-jurisdictional relationships and knowledge of local transportation systems, challenges, and goals for the community. The Questionnaires were tailored to individual organizations to request feedback specific to their needs and knowledge. These stakeholder questionnaires were intended to provide a means of communicating stakeholder concerns early in the Transportation Plan development. The feedback was instrumental in planning the future discussions with the Technical Advisory Committee.

The Stakeholder Questionnaires were sent to the following stakeholders. The stakeholders who provided a response are marked with an (*), and are included in Appendix A.

- * Jerry Keane - Superintendent of Post Falls School District
- * Kimberly Hobson - Kootenai County Transit
- * Joe Jovick - Kootenai County Sheriff's Office
- * Warren Merritt - Kootenai County Fire and Rescue
- * Brian Wallace - Coeur d'Alene School District
- * Phillip Cummings - Coeur d'Alene Airport
- * Chris Bosley - City of Coeur d'Alene
- Alan Soderling - City of Hayden
- Kevin Jump - City of Rathdrum
- Bill Melvin - City of Post Falls
- Bill Roberson - Idaho Transportation Department
- Cathy Mayer - Kootenai Solid Waste
- Glenn Miles - Kootenai Metropolitan Planning Organization
- David Callahan - Kootenai County Community Development
- Darrell Rickard - Lakeland School District,
- MacLennan - North Idaho Centennial Trail Foundation.

For the most part, the responses indicated stakeholder concerns about congestion, safety, the addition of bicycle and pedestrian routes, and railroad crossing safety. The rating of the PFHD existing transportation system by the stakeholders was generally fair to good. The general consensus was that

the District's safety was fair to good, congestion was fair to good, truck traffic was fair, traffic operations (i.e. traffic signal timing/coordination) was fair to good, maintenance was fair to good, and bicycle and walkability was poor to fair. The issues identified as most important to address in the Transportation Plan were safety and maintenance. The concepts deemed most important for transportation planners to concentrate on were reducing fatalities and injuries, reducing congestion, and maintaining current infrastructure.

Technical Advisory Meeting #1

The first Technical Advisory Committee (TAC) Meeting was held on August 17, 2017 from 9:00 to 11:00am at the PFHD offices. The TAC participating members for Meeting #1 were:

- Terry Werner – PFHD Commissioner
- Todd Tondee – PFHD Commissioner
- Lynn Humphreys – PFHD Commissioner
- Kelly Brownsberger – PFHD Road Supervisor
- David Callahan – Director of Kootenai County Community Development
- Chris Bosley – City of Coeur d'Alene
- Alan Soderling – City of Hayden Engineer
- Bill Melvin – City of Post Falls Engineer
- Glenn Miles – Kootenai Metropolitan Planning Organization
- Eric Shanley – Lakes Highway District Engineer
- Steven Kjergaard – Coeur d'Alene Airport
- Mike Fuller – Coeur d'Alene Pedestrian & Bicycle Advisory
- Darius Ruen – PFHD Engineer
- Stefani Mason – Ruen-Yeager Engineer
- Laura Winter – Ruen-Yeager Engineer

The purpose of a Technical Advisory Committee is to assemble a team of leaders from the local transportation community who can provide insight and recommendations that stem from their unique experiences and expertise. There was a roundtable discussion concerning the four topics listed below. The discussion from the Technical Advisory Meeting #1 laid the groundwork for the development of the Capital Improvement Plan and areas of emphasis within the Transportation Plan. The discussions for each topic is listed below and summarized in Appendix B.

Areas of Safety Concerns

- Highway 53 intersections
- Railroad Crossings
- Bus Stops
- Riverview Drive - Bike Safety
- Lancaster & Huetter Intersection
- Lancaster & Meyer Intersection
- Lancaster & SH-41 Intersection
- Huetter Bike Facility

Bike/ped on Seltice
Huetter Bypass
Pleasant View & Prairie Intersection
Bike Connectivity throughout the District

Areas with active or expected growth or change

Lancaster Road - 70 + 90 lots (and could see another 100 lots)
Pleasant View Road & Beck Road
Atlas Road/Huetter Road/Lancaster Road
Hanley Road/Poleline Avenue @ Huetter Road
Prairie Avenue / SH-41
SH-41 Corridor, especially Horsehaven Avenue

PFHD Strengths and Areas for Improvement

Strengths

Participation in Regional Growth & Collaboration Efforts
Active solving of Safety Concerns within the district

Areas for Improvement

Increase communication with residents
Send out mailers for areas with upcoming construction
KMPO has developed a website to update regional construction the District could link to
Media Outreach - Facebook, Twitter

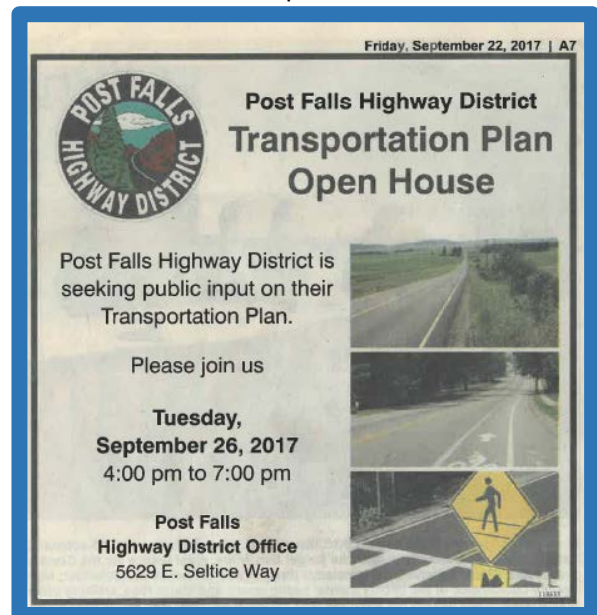
Proposed Future Projects

Greensferry Bridge

Public Open House #1

The first Public Open House was held on September 26, 2017 from 4:00 to 7:00 pm at the Post Falls Highway District office. The purpose of the first Public Open House was to explain to the Public what a Transportation Plan is, provide information concerning the District's current Capital Improvement Plan, present areas of safety concerns, and to provide a question/answer discussion of concerns and issues the public had with the District's roadway system.

The advertisement effort for the Open House included invitations to stakeholders, TAC members, and neighboring jurisdictions, flyers posted at local libraries, city halls, and grocery stores, public service announcements on several local radio stations, advertisement in the Coeur d'Alene Press, Craigslist announcement, and announcement at the KMPO meeting.



The event was well attended with over forty visitors. There were six exhibits, the first explaining “What is a Transportation Plan?”, a PFHD Map with the current CIP improvements identified, a PFHD Map available for marking any hot spots or areas of concern, a PFHD Map identifying the top ten crash locations, a Kootenai County Map identifying Bike and Pedestrian facilities, and an interactive LHTAC Crash Map was on display for questions about specific areas. There were seven total PFHD representatives (two Commissioners, the Road Supervisor, and four Ruen-Yeager staff) present to answer questions and explain the exhibits.



Most of the visitors expressed an appreciation for the Highway District’s dedication and hard work. There were many questions about the future prospects of rebuilding the Greensferry Bridge, with most in support and few opposed. There were many requesting improvements along Prairie Avenue.

A Public Open House Questionnaire was given to each attendee with encouragement to provide a response. Not all attendees responded, and several couples provided a joint response. A total of 24 written responses were gathered with the most consistent concerns being:

- Maintaining infrastructure
- Adding new roads or capacity to existing
- Safety
- Prairie Avenue improvements to intersections and capacity
- Connectivity to I-90 and SH-53
- Developing bike and pedestrian facilities
- Increasing Transit Accessibility
- Building the Greensferry Bridge

The feedback from the public at the Open House provided the public's unique viewpoint with concerns and questions sometimes not realized by the transportation community. Much of the proposed projects on the District's Capital Improvement Plan had the support of the Public. There was support and opposition to the increasing use of roundabouts. Multiple requests for improvements on Prairie Avenue were received. There was concern about the future Pleasant View and SH-53 interchange and the resulting traffic funneling after the closure of McGuire Road and Prairie Road at SH-53. The Public's desire for expanded bike and pedestrian facilities was clearly made in the written responses and verbal discussions. A Summary of the Public Open House #1 Questionnaire responses is included in Appendix C, including the individual responses, and the Open House Flyer used for advertisement.

Technical Advisory Meeting #2

The second Technical Advisory Meeting was held on November 2, 2017 from 9:00 to 11:00am at the PFHD offices. The TAC participating members for Meeting #2 were:

- Terry Werner – PFHD Commissioner
- Todd Tondee – PFHD Commissioner
- Kelvin Brownsberger – PFHD Road Supervisor
- Chris Bosley – City of Coeur d'Alene
- Bill Melvin – City of Post Falls Engineer
- Ali Marienau – Kootenai Metropolitan Planning Organization
- Eric Shanley – Lakes Highway District Engineer
- Darius Ruen – PFHD Engineer

The purpose of the second TAC meeting was to rank the proposed projects with a category matrix. The sum of the categories then ranked the importance of a project with a score between 0 and 100, with 100 being the highest priority. These scores then established the sequence of project scheduling in the Five-Year CIP plan. This process provides a fully vetted CIP Plan, with systematic scheduling.

The projects were reviewed, and the ranking categories discussed. It was decided by the group that the projects in the CIP with previously secured funding and a definitive schedule would not be included in the priority ranking, since these projects already had an established schedule of design and construction. The group also concluded that the ranking categories should be revised to provide a more

accurate ranking. The categories were reviewed and discussed, with the outcome providing some categories removed and/or added, and ranking points were revised. The initial and final ranking categories with maximum points are listed below in descending order of importance:

Initial Ranking Categories

- 20 - Safety
- 15 - Remaining Service Life
- 15 - Ties to Adjacent Projects
- 15 - Area Growth
- 10 - Project Cost
- 10 - Funding Source
- 05 - Public Support
- 05 - Right-of-Way Needs
- 05 - Utility Impacts
- 100 – Maximum Points

Final Ranking Categories

- 25 - Safety
- 15 - Transportation/Capacity
- 15 - Economic Vitality
- 15 - Remaining Service Life
- 10 - Public Support
- 10 - Ties to Adjacent Projects
- 05 - Project Cost
- 05 - Right-of-way Needs
- 100 – Maximum Points

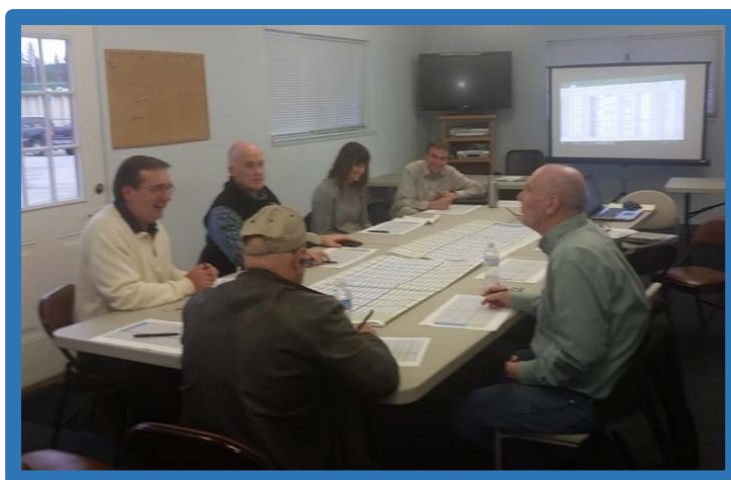
The Initial and Final CIP Project Ranking Matrices are provided in Appendix D. A third TAC meeting to finalize the ranking of the projects was scheduled for after the Thanksgiving holidays.

Technical Advisory Meeting #3

The third Technical Advisory meeting was held on December 12, 2017 from 9:00 to 11:00 am at the PFHD offices. The TAC participating members for Meeting #3 were:

- Terry Werner – PFHD Commissioner
- Todd Tondee – PFHD Commissioner
- Chris Bosley – City of Coeur d’Alene
- Bill Melvin – City of Post Falls Engineer
- Ali Marienau – Kootenai Metropolitan Planning Organization
- Eric Shanley – Lakes Highway District Engineer
- Darius Ruen – PFHD Engineer
- Laura Winter – Ruen-Yeager Engineer

The purpose of this meeting was for each TAC member to individually rank the proposed projects for the CIP plan based on the Final CIP Project Ranking Matrix modified in the second meeting. There was limited discussion concerning the projects, as the intent of this meeting was to secure independent scores for the projects from each TAC member. The individual ranking scores were then averaged to determine the priority ranking of the projects in the proposed Five-Year CIP plan.



The results of the project rankings were as follows:

74.2	Prairie Avenue – Five Lanes from Meyer to SH-41	Road Improvement
63.9	Prairie Avenue & Greensferry Road	Intersection Improvement
63.8	Pleasant View and SH-53	Intersection Improvement
57.0	Prairie Avenue - SH-41 to Greensferry	Road Improvement
55.3	Greensferry Bridge	Bridges/Culverts
52.4	Hayden Avenue & Meyer Road	Intersection Improvement
46.4	Huetter Road Bikeway	Bike Ped Improvements
44.7	Seltice Way – Seeley to Huetter	Road Improvement
39.8	Seltice Way Connection to Centennial Trail	Bike Ped Improvement
35.4	Riverview Drive – at Idaho Road	Intersection Improvement
35.2	Riverview Drive – Curve Realignment	Road Improvement
32.6	Riverview Drive Extension	Road Improvement
28.4	Upriver Drive – Realignment at Jacobs Loop	Road Improvement
21.9	Riverview Drive & Harbor Drive	Intersection Improvement

A full exhibit of the Final CIP Project Ranking Matrix with the averaged scores is included in Exhibit E.

Public Open House #2

The second Open House was held on January 9, 2017 from 4:00 to 7:00 pm at the Post Falls Highway District Office. The purpose of the second Open House was to present the ranking of projects in the Proposed CIP Plan and gather public input.

The advertisement effort was similar to the first Open House, including invitations to the Stakeholders, TAC members, and neighboring jurisdictions, flyers posted at local libraries, city halls, and grocery stores, public service announcements on several local radio stations, Craigslist announcement, and advertisement in the Coeur d’Alene Press.

The event was again well attended with over 40 visitors. There were two exhibits, the first displaying the Scored Final CIP Project Ranking Matrix and the second identifying the locations of these projects on a Post Falls Highway District map.

There were six total PFHD representatives (two Commissioners, the Road Supervisor, and three Ruen-Yeager staff) present to answer questions and explain the exhibits.

A Public Open House Questionnaire was given to each attendee with encouragement to take the time to provide a response. Not all attendees responded, with many couples, providing a joint response. A total of twenty-four written responses were gathered with the most consistent comments being:

- Greensferry Bridge with nine comments directly in favor and two opposed
- Appreciation for the good job PFHD does
- Comments that the rankings seemed to appear in a reasonable order
- Happy to see Bike/Ped projects on the forecast

A Summary of the Public Open House #2 Questionnaire responses is included in Appendix F, including the individual responses, the front-page article from the Coeur d'Alene Press, and the Open House Flyer used for advertisement.



Public Input Analysis

Through the public outreach process, comments were received from the public that merited further analysis. The development of the PFHD Transportation Plan considers the needs of the community a critical component in developing a plan that addresses the needs and concerns of all users.

The input from the public was included in the analysis prepared for the existing conditions in terms of AADT, crash data, growth factors, land use changes, and other analysis performed in developing the Transportation Plan and the CIP Plan.

Land Use & Growth Impacts

An important component of the development of the Transportation Plan is to look at existing and future land use to predict where growth may impact traffic volumes. This effort helps roadway improvements to occur before or as growth is happening, rather than after congestion has already taken affect.

The local municipalities adjacent to Post Falls Highway District have jurisdiction over their land use zoning. The Post Falls Highway District does not have land use authority over its jurisdiction. Kootenai County has the land use zoning and comprehensive planning authority of the rural areas outside of the municipal jurisdictions, which includes the Post Falls Highway District jurisdiction. Changes to land use and zoning can create impacts to the demand on the PFHD roadway system. These zoning and land use impacts were considered for the development of this Transportation Plan. A map of the current zoning from Kootenai County is shown in Figure 3.

There is varied zoning in the District. With the District's unique location, bordered by Washington State to the west, Rathdrum to the north, and Post Falls, Hayden and Coeur d'Alene along the perimeters, the expectation of future growth is quite certain.

Existing Land Use

The District currently has much of its jurisdiction across the Rathdrum Prairie classified as Agriculture, Light Industrial, or Mining. The District north of SH-53 is primarily Rural, Ag-Suburban, and Upper Watershed to Hauser Lake. The District's jurisdiction south of I-90 is primarily Rural and Ag-Suburban.

A general description for the primary zoning designations within the District are as follows:

Agriculture – The Agricultural zone is a zoning district in which the land has been found to be suitable for uses related to farming, agriculture, forestry, silviculture, aquaculture, and other similar uses. The minimum lot size for parcels in the Agriculture zone is five (5.00) acres.

Light Industrial – The Light Industrial zone is a zoning district in which the land has been found to be suitable for manufacturing and processing of a non-nuisance character. The purpose of the Light Industrial zone is to encourage the development of manufacturing and wholesale businesses that are clean, quiet, and free of noise, odor, dust, and smoke.

Mining – The Mining zone is a zoning district in which the land has been found to be suitable for excavation and processing materials secured from the earth.

Rural – The Rural zone is a zoning district in which the land has been found to be suitable for rural residential uses and uses related to agricultural pursuits, including farming and forestry. The minimum lot size for parcels in the Rural zone is five (5.00) acres.

Ag-Suburban – The Agricultural Suburban zone is a zoning district in which the land has been found to be suitable for residential and small-scale agricultural uses. The minimum lot size for parcels located in the Ag-Suburban zone is two (2.00) acres.

Upper Watershed – The Upper Watershed zone north of Hauser Lake has a minimum lot size of five (5.00) acres per parcel.



POST FALLS HIGHWAY DISTRICT 2018 Transportation Plan Kootenai County Zoning Map

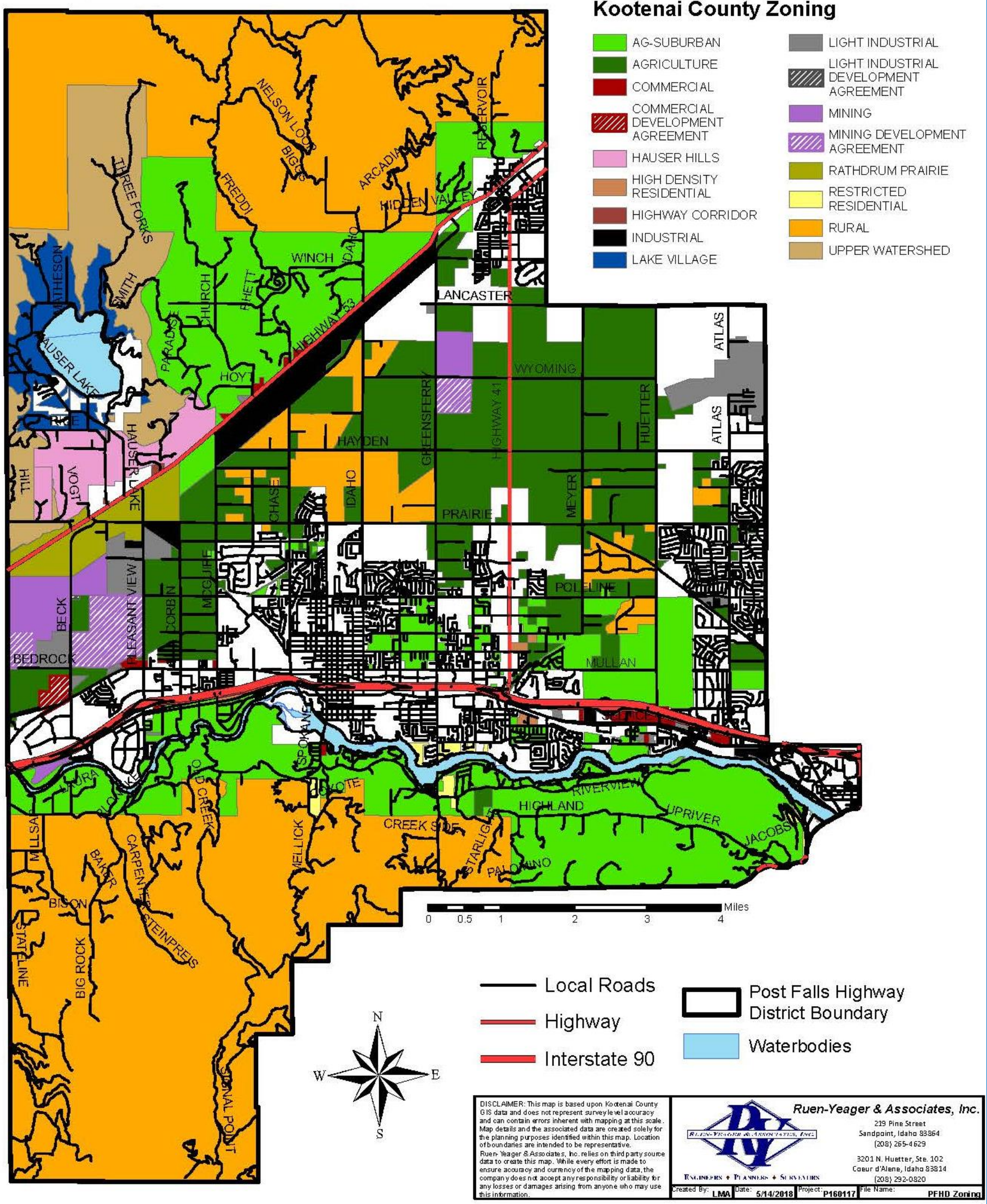


Figure 3 – Current Land Use Zoning Map

Development Activities

While the Rathdrum Prairie is expected to experience continued development, all of the neighboring municipalities are experiencing growth as well, with traffic impacts currently being felt in the District. The Kootenai Metropolitan Planning Organization (KMPO) has developed an annual growth rate projection for all of Kootenai County and lists the projected annual growth rate for Post Falls Highway District at 0.217%. The KMPO has also produced a Population Density Map to identify the density in Kootenai County as of 2016 (see KMPO population data in Appendix G). Though this may seem low, the projections for the surrounding municipalities indicate significantly higher growth, as listed below. Growth experienced in these neighboring jurisdictions are felt in the PFHD as motorists utilize the District's roadway system.

Post Falls Highway District – 0.21% annual growth rate

City of Post Falls – 4.801% annual growth rate

City of Rathdrum – 3.549% annual growth rate

City of Hayden – 3.796% annual growth rate

City of Coeur d'Alene – 2.499% annual growth rate

There were several development activities identified with a potential to impact the District's roadway system.

Lancaster Road west of US-95 – Hayden North Village is a new development with nearly 300 residential high-density units and homes.

Pleasant View Road and Beck Road - Mining and Light Industrial growth.

Prairie Avenue and SH-41 – Residential growth impacts are expected to be evident at this major intersection located within the state system.

Hanley Road at Huetter – Future expansion of the residential area will extend Hanley Road to intersect with Huetter Road at Poleline Avenue.

SH-41 Corridor – Residential growth along the expanse of the SH-41 corridor, especially along Horsehaven Avenue.



Hayden North Village on Lancaster Road

Projected Land Use

As growth occurs in the District, it is anticipated that growth will align with the Kootenai County Land Use Map as seen in Figure 4. The map presents a significant portion of the Rathdrum Prairie as incorporated either into the City of Post Falls, Rathdrum, Hayden, or City of Coeur d’Alene. The remainder of the Rathdrum Prairie is depicted as transitional, which is a category designation to reserve land for future annexation into incorporated areas. The District areas just south of the Spokane River are identified as suburban, which is in line with current conditions.

Population Demographics

The current population data from the 2010 US Census is shown below with a twenty-year growth factor. The table shows the 2010 census population and the 1990 to 2010 population annual growth trend in Kootenai County unincorporated areas and urban areas.

Population Trend in Kootenai County

	1990 Census Population	2010 Census Population	1990 - 2010 Annual Growth Rate
Kootenai County Urban Area	42,047	98,822	4.40%
Kootenai County Unincorporated Areas	27,748	39,672	1.80%
Total	69,795	138,494	3.50%

Source: US Census Bureau

Though the growth trends for the unincorporated areas in the District are not as high as the urban areas, those urban areas utilize the District’s roadway system and are increasing the volume of traffic that must be addressed by the District. The KMPO annual growth projections indicate the PFHD’s jurisdictional population will increase to 11,082 by the year 2020, and to 11,448 by year 2035. In addition, the same projections indicate Kootenai County’s population to increase to 178,280 by the year 2020, and to 273,566 by year 2035. Another factor to consider is these growth projections are based on 1990 to 2010 census data, and do not take in to account the current growth expansion Kootenai County is experiencing now. At the start of 2018, the City of Post Falls was planning for an annual 5% increase in population growth for the coming years.

Age and Income Demographics

The Census data from 1990 to 2010 provided the historic and current age demographics for Kootenai County. The data shows the median age for Kootenai County residents has risen from a median age of 35 in 1990, to 36.1 in 2000, and 38.5 in 2010, compared to the State of Idaho at 31.5, 33.2 and 34.6 for the same respective years.

The Census and the US Department of Health and Human Services data estimated the median household income in Kootenai County at \$49,151 in 2010, compared to the State of Idaho median household income at \$47,015. This same data provided an estimate of 12.8% of persons living below poverty in Kootenai County, compared to 15.1% of persons living below poverty in the State of Idaho.

Summary of Population and Traffic Forecasts

While the future population growth of the District is estimated to increase at an annual rate of 0.217%, the KMPO projected the annual growth rate of Kootenai County between now and 2035 will average 2.4%, and the urban areas within Kootenai County will grow at an annual growth rate of 3.253%, or higher based on current growth trends. These growth percentages help forecast traffic volumes and determine design life of transportation infrastructure improvements. When looking at these percentages and converting them to population numbers, the annual growth trend indicates that the District will add 604 to their population, the urban areas surrounding the District will add 132,408 population, and Kootenai County will add 135,072 to its population by the year 2035.



POST FALLS HIGHWAY DISTRICT

2018 Transportation Plan

Kootenai County Comprehensive Plan Map

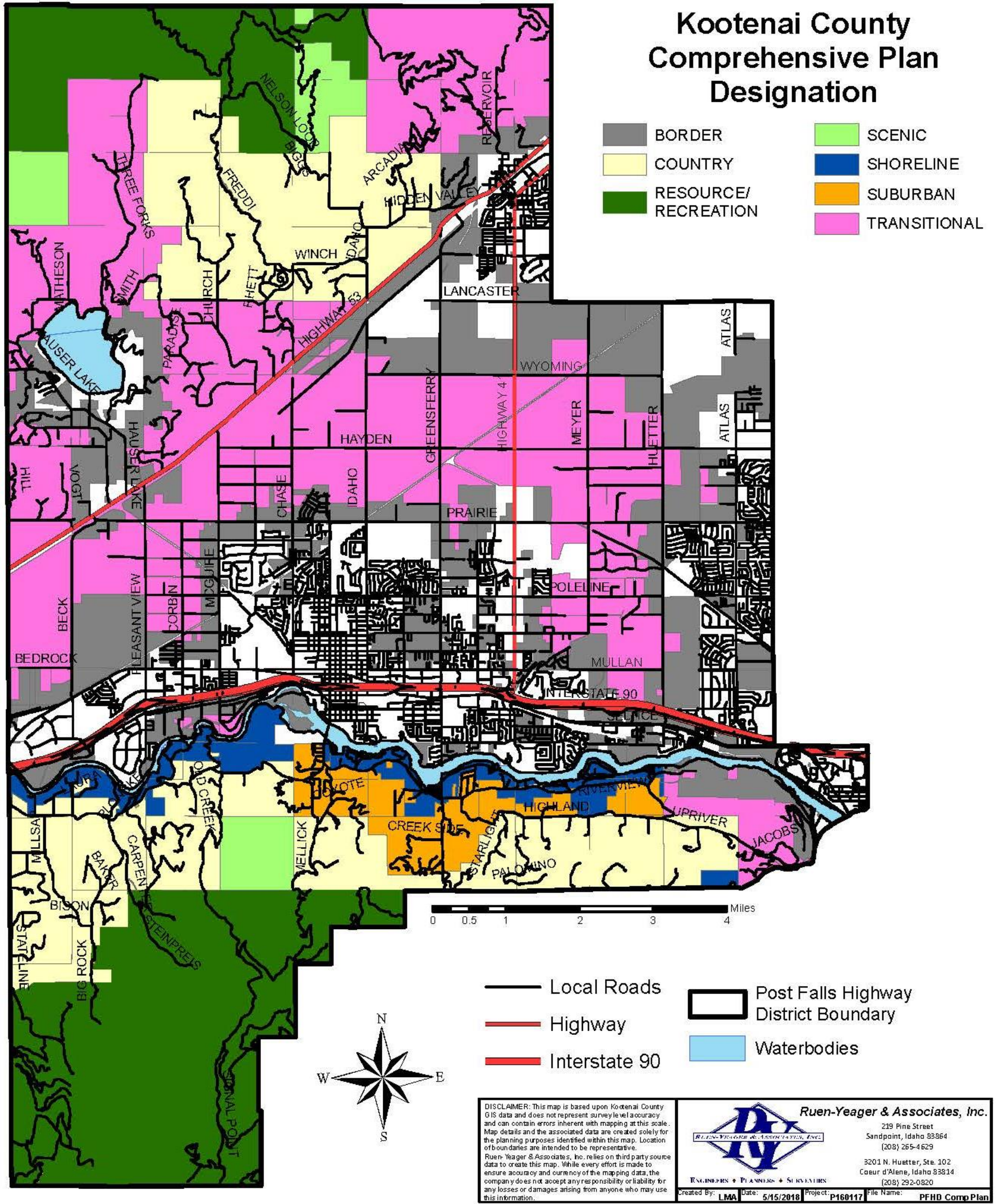


Figure 4 – Kootenai County Comp Plan Designation in PFHD

Existing Conditions and Future Plans

In the development of the PFHD Transportation Plan, existing published transportation plans from neighboring jurisdictions were gathered and reviewed. By taking into consideration the transportation infrastructure improvements of neighboring jurisdictions, the PFHD Capital Improvement Plan was designed to coincide and link with outside projects when possible and feasible. Regional Plans collected in this effort include:

- Kootenai County Comprehensive Plan
- Kootenai Metropolitan Planning Organization
 - Transportation Plan
 - SH-41 Corridor Master Plan
 - Huetter Corridor Study
- City of Hayden Transportation Strategic Plan Update
- Lakes Highway District Transportation Plan
- City of Coeur d'Alene Comprehensive Plan
- City of Post Falls Transportation Plan
- Worley Highway District Transportation Plan

The PFHD is a member of the Kootenai County Area Transportation Team (KCATT) which meets monthly at ITD with other local highway districts and municipalities to discuss and coordinate local projects. The PFHD will continue to coordinate and follow the improvements being made by ITD and other neighboring jurisdictions.

The Idaho Transportation Department (ITD) has two significant improvements that could impact the PFHD roadway system, and spur improvements within the Highway District. The first ITD project is the improvements planned for SH-41. This is a state highway, with intersecting streets from Prairie Avenue to Lancaster Road belonging to PFHD. The improvements include widening the highway to a four-lane highway, installing intersection improvements, improving stormwater facilities and adding a separated bicycle and pedestrian path. The PFHD will continue to monitor these improvements closely and will plan for improvements as necessary to their adjoining streets.

The second ITD project is the planned improvements to SH-53 from the Washington State line to west of Rathdrum. This development is in the preliminary phase and is anticipated to provide a center turn lane and right turn bays, as well as illumination at intersections. Though SH-53 is a State Highway, many of the intersections are PFHD local roads. The PFHD will continue coordination with the Idaho Transportation Department to plan for improvements within the PFHD that adjoin SH-53.

Roadway Network Functional Classifications Review

The District, KMPO, in coordination with the Kootenai County Area Transportation Team (KCATT) members, collaboratively updated the Urban and Rural Federal Functional Classification Maps for Kootenai County. The most current update was performed in 2013, with recommended changes

approved by the KMPO Board in December of 2013, and approval by Federal Highways Administration (FHWA) in 2014 (see Appendix H).

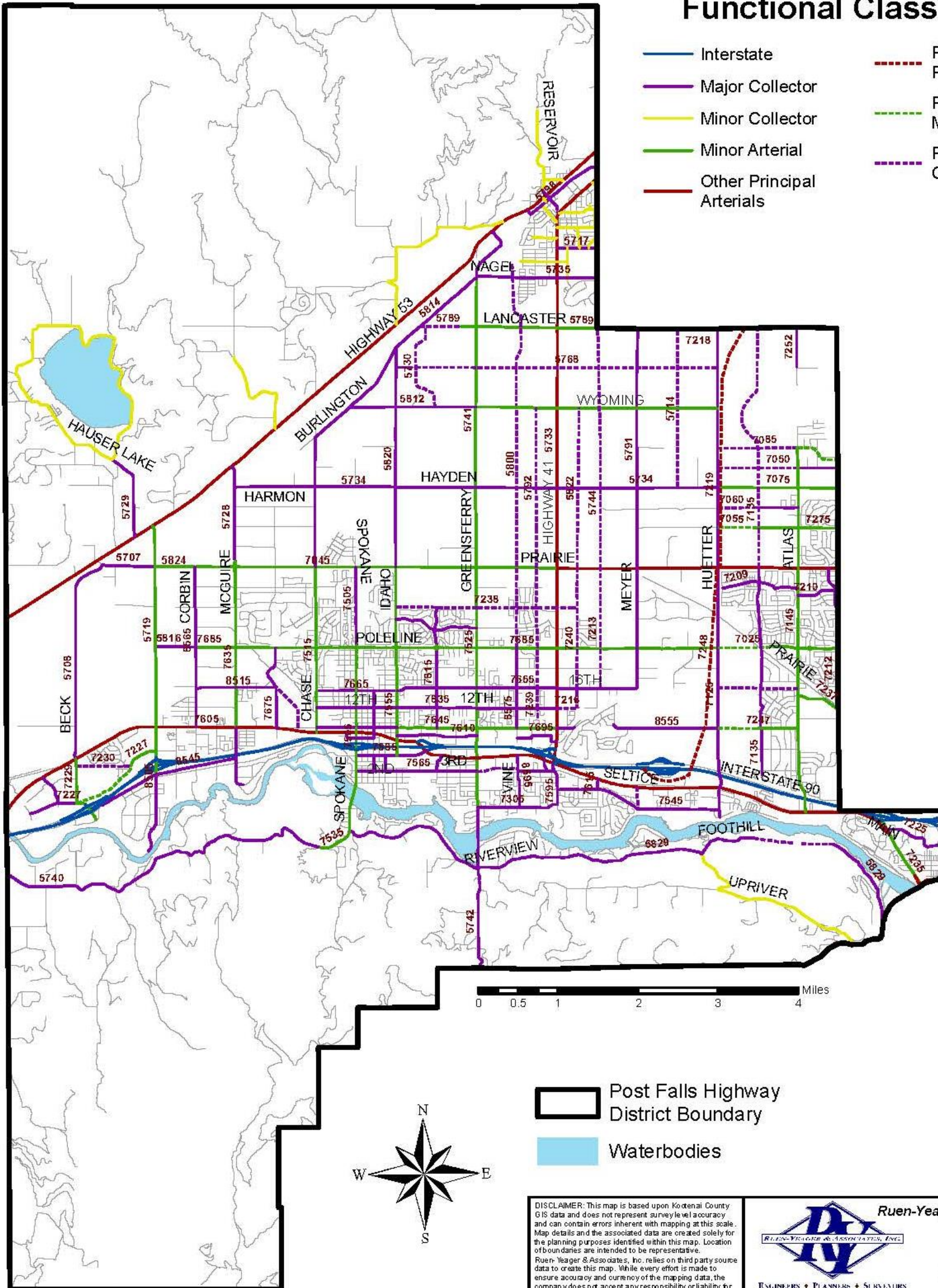
During the course of the Transportation Plan development, the functional classifications in the PFHD jurisdiction were reviewed to determine if any road classifications could have changed. After thorough review, it was determined no changes have occurred since FHWA approval in 2014. There was one minor correction that should be brought to the KMPO's attention, the Huetter Bypass Corridor has a line type indicating it is an existing Other Principal Arterial, when it is should be marked as a future Other Principal Arterial. The KMPO Rural and Urban Federal Functional Classification Maps are attached in Appendix H. A PFHD jurisdictional map with the ITD Roadway Classifications is shown on the next page.



POST FALLS HIGHWAY DISTRICT 2018 Transportation Plan ITD Functional Classification Map

Functional Classification

- Interstate
- Major Collector
- Minor Collector
- Minor Arterial
- Other Principal Arterials
- Proposed Other Principal Arterials
- Proposed Urban Minor Arterial
- Proposed Major Collector



Post Falls Highway District Boundary

Waterbodies

DISCLAIMER: This map is based upon Kootenai County GIS data and does not represent survey level accuracy and can contain errors inherent with mapping at this scale. Map details and the associated data are created solely for the planning purposes identified within this map. Location of boundaries are intended to be representative. Ruen-Yeager & Associates, Inc. relies on third party source data to create this map. While every effort is made to ensure accuracy and currency of the mapping data, the company does not accept any responsibility or liability for any losses or damages arising from anyone who may use this information.

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Created By: **LMA** Date: **5/15/2018** Project: **P160117** File Name: **PFHD Functional Classification**

Figure 5 – Post Falls Highway District Functional Classification Map

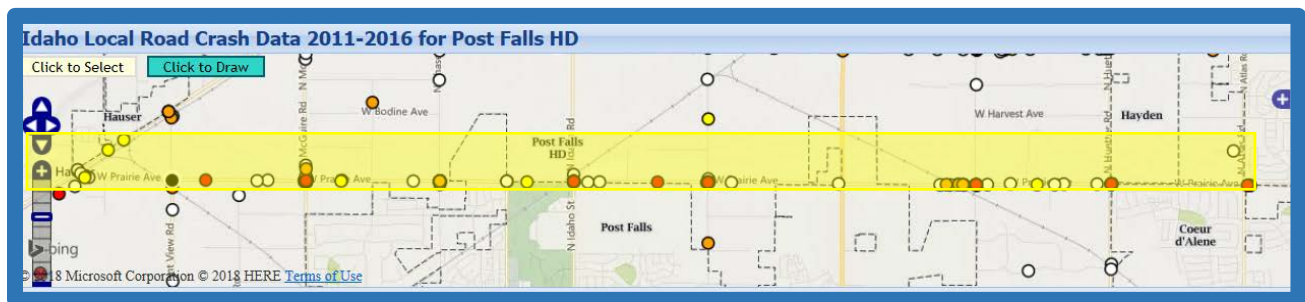
The FHWA defines each functional classification for both rural and urban, with the Associated Highway Districts guidelines for each functional classification as follows:

Urban Federal Functional Classification	FHWA Definition
Urban Other Principal Arterial	<p>Serve major activity centers, highest traffic volume corridors and longest trip demands</p> <p>Carry high proportion of total urban travel on minimum of mileage</p> <p>Interconnect and provide continuity for major rural corridors to accommodate trips entering and leaving urban area and movements through the urban area</p> <p>Serve demand for intra-area travel between the central business district and outlying residential areas</p>
Urban Minor Arterial	<p>Interconnect and augment the higher-level arterials</p> <p>Serve trips of moderate length at a somewhat lower level of travel mobility than Principal Arterials</p> <p>Distribute traffic to smaller geographic areas than those served by higher-level arterials</p> <p>Provide more land access than Principal Arterials without penetrating identifiable neighborhoods</p> <p>Provide urban connections for Rural Collectors</p>
Urban Major Collector	<p>Serve both land access and traffic circulation in <i>higher</i> density residential, and commercial/industrial areas</p> <p>Penetrate residential neighborhoods, often for significant distances</p> <p>Distribute and channel trips between Local Roads and Arterials, usually over a distance of greater than three-quarters of a mile</p> <p>Operating characteristics include higher speeds and more signalized intersections</p>
Urban Minor Collector	<p>Serve both land access and traffic circulation in lower density residential and commercial/industrial areas</p> <p>Penetrate residential neighborhoods, often only for a short distance</p> <p>Distribute and channel trips between Local Roads and Arterials, usually over a distance of less than three-quarters of a mile</p> <p>Operating characteristics include lower speeds and fewer signalized intersections</p>

Rural Federal Functional Classification	FHWA Definition
Rural Other Principal Arterial	<p>Serve corridor movements having trip length and travel density characteristics indicative of substantial statewide or interstate travel</p> <p>Connect all or nearly all Urbanized Areas and a large majority of Urban Clusters with 25,000 and over population</p> <p>Provide an integrated network of continuous routes without stub connections (dead ends)</p>
Rural Minor Arterial	<p>Link cities and larger towns (and other major destinations such as resorts capable of attracting travel over long distances) and form an integrated network providing interstate and inter-county service</p> <p>Be spaced at intervals, consistent with population density, so that all developed areas within the State are within a reasonable distance of an Arterial roadway</p> <p>Provide service to corridors with trip lengths and travel density greater than those served by Rural Collectors and Local Roads and with relatively high travel speeds and minimum interference to through movements</p>
Rural Major Collector	<p>Provide service to any county seat not on an Arterial route, to the larger towns not directly served by the higher systems and to other traffic generators of equivalent intra-county importance such as consolidated schools, shipping points, county parks and important mining and agricultural areas</p> <p>Link these places with nearby larger towns and cities or with Arterial routes</p> <p>Serve the most important intra-country travel corridors</p>
Rural Minor Collector	<p>Be spaced at intervals, consistent with population density, to collect traffic from Local Roads and bring all developed areas within reasonable distance of a Collector</p> <p>Provide service to smaller communities not served by a higher class facility</p> <p>Link locally important traffic generators with their rural hinterlands</p>

Traffic Safety Concerns

A review of the LHTAC Local Road Crash Data Map identified key crash locations, or hot spots. Also taken into consideration was public comments concerning areas with safety concerns to the public. The area with the most focus was Prairie Avenue. Of the top ten crash locations in PFHD, seven of them are intersections along Prairie Avenue. The LHTAC Local Road Crash Data Map is shown below with Prairie Avenue highlighted in yellow.



Prairie Avenue Crash Locations – LHTAC Interactive Crash Map

Of the seven Prairie Avenue intersections, three experienced recent intersections improvement completed by PFHD. The Huetter Road and Meyer Road intersections recently were converted from two-way stop to signalized intersections. The McGuire Road intersection was converted from a two-way stop to a roundabout. Two of the intersections will soon be converted to roundabouts, with the Prairie-Chase Roundabout construction in 2018 and Prairie-Pleasant View roundabout in design and scheduled for construction in 2020. The remaining two intersection hotspots on Prairie Avenue at both Idaho Road and Greensferry Road are currently two way stops and are in the CIP for conversion to a signalized intersection. Greensferry Road intersection is currently in preliminary design, and Idaho Road was listed as an approved project for LHSIP funding in 2021.

The other three projects in the top ten crash locations are Huetter Road at Seltice Way, Wellesley Road at Seltice Way, and Pleasant View Road at Seltice Way. The Huetter Road and Seltice Way Intersection was recently converted from a two way stop to a signalized intersection in late 2016. Twenty-three of the twenty-four accidents listed on the LHTAC crash map for this intersection occurred prior to the signalization upgrade. As future crash data is populated into the LHTAC crash map, it will be evident whether greater intersection safety has been achieved. The Wellesley Avenue and Seltice Way intersection logged 15 accidents between 2011 and 2014. There have been no accidents at this intersection since October of 2014 when safety improvements to this intersection were made by the District and Spokane County. The Pleasant View and Seltice Way signalized intersection has logged twenty-three accidents between 2011 and 2016. PFHD will look at any safety improvements that may be warranted at this intersection.

Inter-Mode Transportation Facilities Inventory

Through coordination with KMPO and the Kootenai County Transit, a review was performed of the inter-modal facilities within the PFHD. These include:

Transit

Kootenai County Transit operates three bus routes in the Kootenai County Metropolitan Area. Currently, the “B-Route” is the only route which operates within the PFHD jurisdiction (see Figure 6 – Kootenai County Transit “B-Route”). The “B-Route” traverses into PFHD along Seltice Way with one stop within the District at Huetter Road. The Kootenai County Transit B-Route map can be found online at www.kcgov.us/departments/transit/transitpdfs/BRoute2.pdf.

During the development of the Transportation Plan, PFHD coordinated with Kootenai County Transit to explore ways to assist with transit facilities. Kootenai County Transit expressed the desire for stakeholder involvement during the design process of road improvements, to expand or improve their transit stops. Further discussion concerning design of transit stops, roundabout pedestrian and bicycle facilities, and providing for snow storage during design indicated a need to include Kootenai County Transit in future preliminary design planning.

Bicycle and Pedestrian Facilities

The Bicycle and Pedestrian community was contacted during the development of the Transportation Plan. The Technical Advisory Committee included Mike Fuller, a member of the Coeur d’Alene Ped & Bike Advisory Committee. Through the Stakeholder Questionnaires and the Public Open Houses, comments were solicited and received concerning the addition of bike and pedestrian facilities.

The Kootenai Metropolitan Planning Organization inventoried and created a map of “Non-Motorized Pathways - Rural, Kootenai County”. The map was first published in the 2010 KMPO “Kootenai County Metropolitan Transportation Plan”. In 2017, the map was updated and provided to PFHD for use in developing their Transportation Plan. The map shows the existing and future shared roadways in PFHD along Prairie Avenue, Riverview Drive, McGuire Road, and in proximity to the City of Hauser (see Figure 7 – Non-Motorized Pathways – Rural Kootenai County). The PFHD jurisdiction does include bike and pedestrian facilities wherever feasible and utilized. There are bike lanes and sidewalks along the improved sections of Prairie Avenue, and sections of Seltice Way. The original KMPO “Non-Motorized Pathways – Rural Kootenai County” can be found online at www.kmpo.net. The updated map shown as Figure 7 has not been published yet.

Based on the response from the community, the PFHD added two projects to the Capital Improvement Plan. One of the two projects is the Huetter Road Bikeway, which is a proposed bike path connecting the Prairie Path to the Prairie Avenue bike lanes along the east side of Huetter Road. The second project is the Seltice Way Connection to the Centennial Trail at Huetter Road.

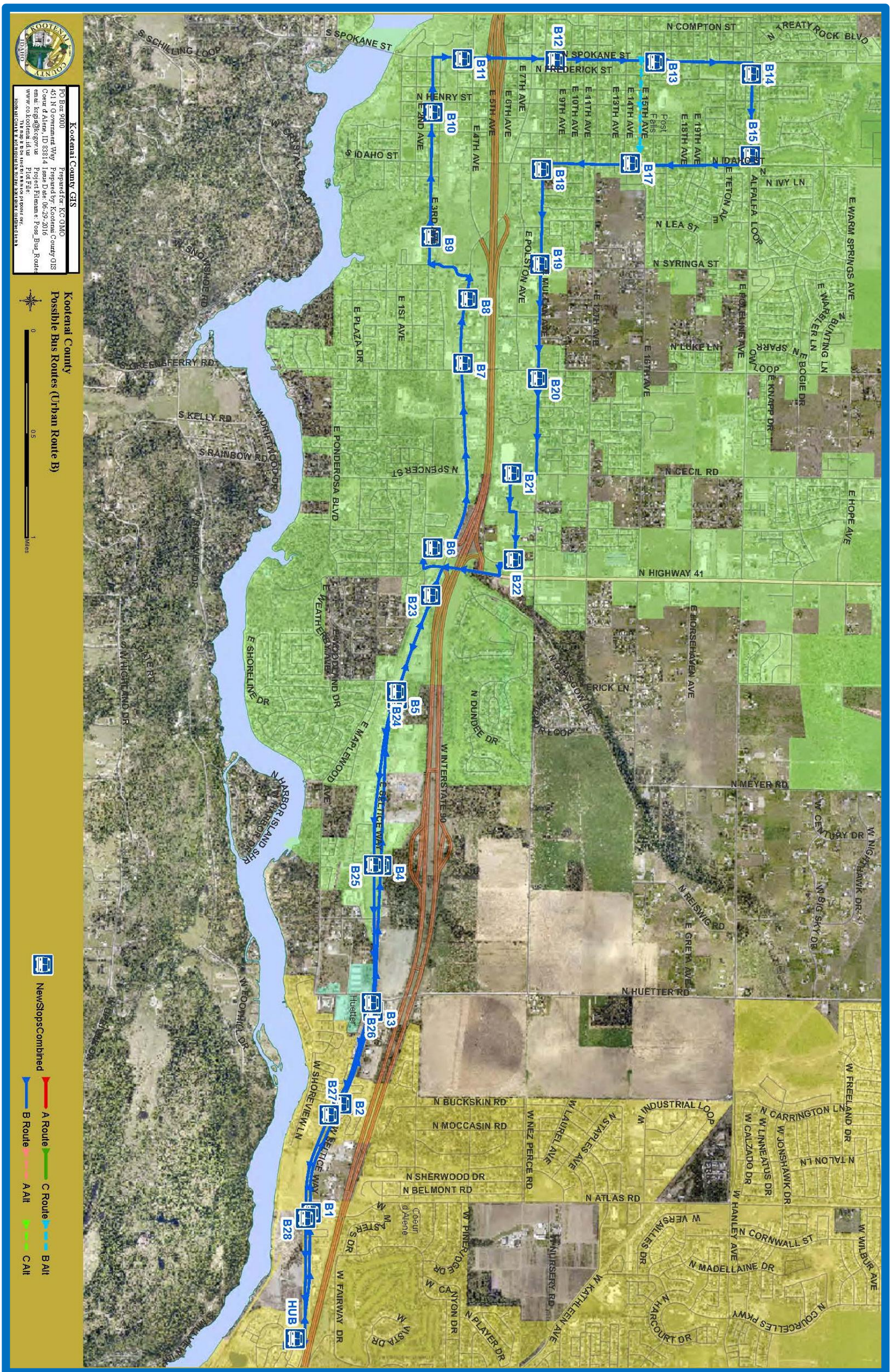
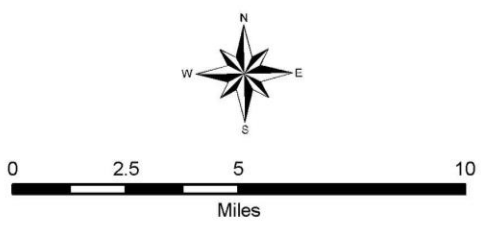
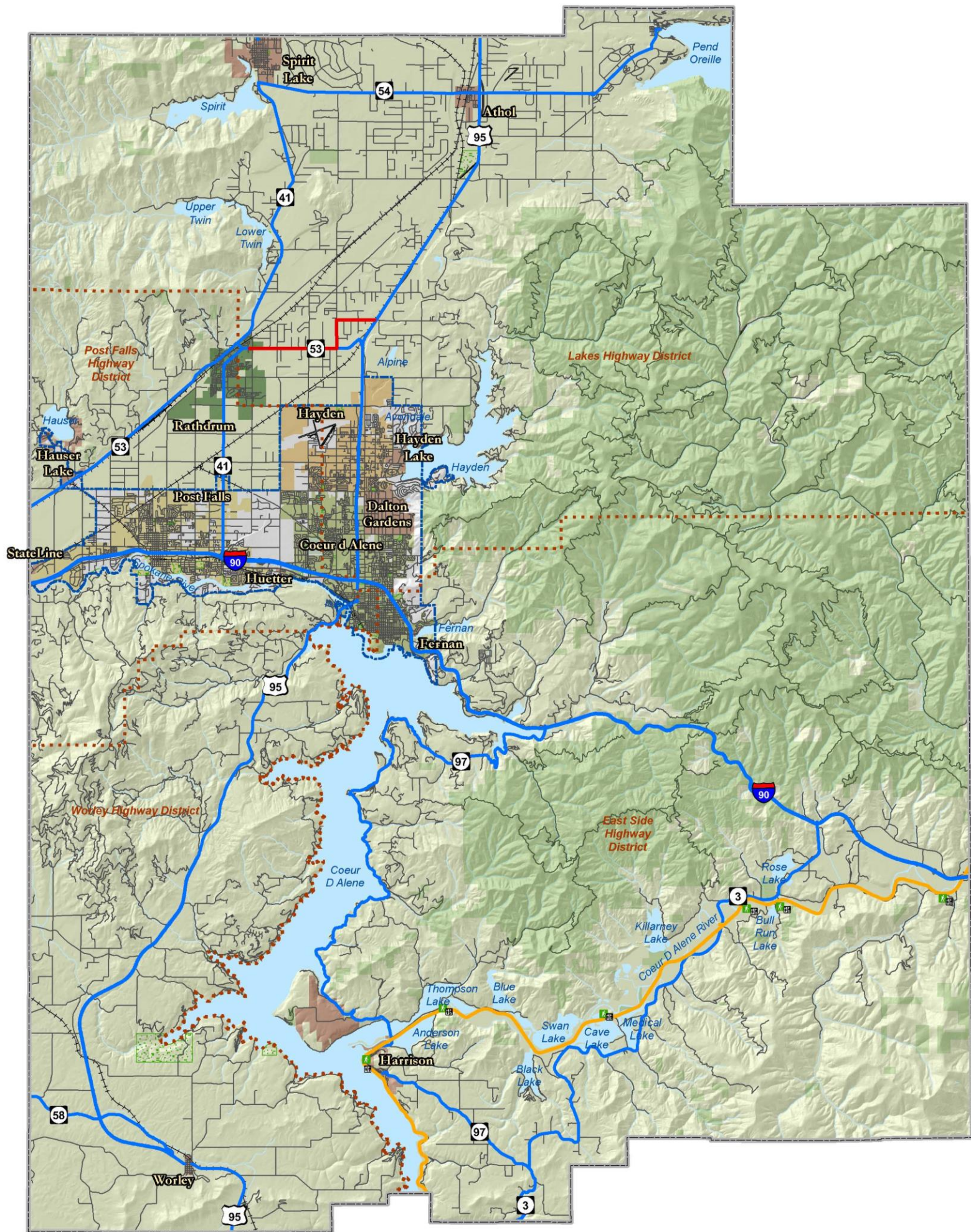


Figure 6 – Kootenai County Transit “B-Route” Map

KOOTENAI METROPOLITAN TRANSPORTATION PLAN
2017-2040



KOOTENAI METROPOLITAN TRANSPORTATION PLAN
2017-2040

**NON-MOTORIZED PATHWAYS
RURAL, KOOTENAI COUNTY**

Pathways & Amenities		Physical Characteristics	
Shared Use Path	Trailhead	Highway Districts	County Boundary
Bicycle Lane	Drinking Fountain	Interstate	Urban Area Boundary
Shared Roadway	Restroom	US/State Highways	National Forests
	Parking	Local/Seasonal Roads	Water Features
		Railroad	Parks

*Data based on best available information. *Data for illustrative purposes only.

Figure 7 – Non-Motorized Pathways – Rural Kootenai County

Airport Facilities

The Coeur d'Alene Airport (COE) borders the PFHD jurisdiction along Huetter Road to the east. The airport provides primarily freight and private transport. The airport is administered by an Airport Board appointed by the Kootenai County Commissioners. As the airport expands its operations to include commercial flights, increased traffic volumes could occur in the PFHD roadway system.

The PFHD continues to coordinate with the Coeur d'Alene Airport concerning their Master Plan and any impacts the airports growth could have on the PFHD roadway system. The Director of the Coeur d'Alene Airport was included on the Technical Advisory Committee to provide feedback specific to the airport concerning the PFHD Capital Improvement Plan development.

Freight & Truck

The PFHD jurisdiction contains agriculture, mining, and light industrial zones which generate truck traffic. In addition, I-90 and SH-53 both transect the PFHD jurisdiction, typically routing trucks along Pleasant View Road. Many of the roads in the District have weight limits posted starting in mid-winter restricting trucks with heavy loads from utilizing these roads. Load limits are usually lifted in the spring, with timing depending on the weather and road conditions. The PFHD does have all-weather roads that are not subject to load limits in the winter. These include Pleasant View Road and Prairie Avenue, which both experience truck traffic.

Rail

There are Union Pacific Railroad (UPRR) lines and Burlington Northern Santa Fe Railroad (BNSF) lines that cross the PFHD jurisdiction. These rail lines typically transport goods.

There are four main track BNSF crossings and seven BNSF Spur crossings. Of the four main track crossings, Greensferry Road was improved to a grade separated crossing, and Pleasant View, McGuire Road, and Prairie Avenue have flashing lights and gates. Pleasant View Road is anticipated to be improved to a grade separated crossing in the near future. When the Pleasant View grade separated improvement is constructed, the intent is to close the railroad crossings at Prairie Avenue and McGuire Road.

There are ten main track UPRR crossings and two UPRR Spur crossings. Most of the main track crossings have flashing lights and gates. There are two UPRR Spur crossings on Prairie Avenue and Meyer Road. Both crossings are planned to be decommissioned in the near future.

The following table lists all the crossings within the PFHD jurisdiction, the crossing treatments, and any planned improvements.

Post Falls Highway District Railroad Crossings

Intersecting Road	Railroad	Existing Infrastructure	Planned Improvements
Prairie Avenue	BNSF	Flashing Lights & Gates	To be closed when Pleasant View Grade Separation is Constructed
Pleasant View Road	BNSF	Flashing Lights & Gates	Grade Separated to be Constructed
McGuire Road	BNSF	Flashing Lights & Gates	To be closed when Pleasant View Grade Separation is Constructed
Greensferry Road	BNSF	Grade Separated Crossing	
Prairie Avenue	BNSF Spur	Railroad Crossing Sign	
Pleasant View Road	BNSF Spur	Flashing Lights & Gates	
N. Corbin Road	BNSF Spur	Railroad Crossing Sign	
W. Grange Avenue	BNSF Spur	Railroad Crossing Sign & Stop Sign	
McGuire Road	BNSF Spur	Flashing Lights & Gates	
Seeley Street	BNSF Spur	Railroad Crossing Sign	
Huetter Road	BNSF Spur	Railroad Crossing Sign	
Beck Road	UPRR	Flashing Lights & Gates	
Pleasant View Road	UPRR	Flashing Lights & Gates	
N. Corbin Road	UPRR	Railroad Crossing Sign & Stop Sign	
Prairie Avenue	UPRR	Flashing Lights & Gates	
Idaho Road	UPRR	Railroad Crossing & Stop Sign	
Greensferry Road	UPRR	Flashing Lights && Gates	
Hayden Avenue	UPRR	Flashing Lights & Gates	
Wyoming Avenue	UPRR	Railroad Crossing Signs & Yield Signs	
Meyer Road	UPRR	Flashing Lights & Gates	
Huetter Road	UPRR	Railroad Crossing & Flashing Stop Sign	
Prairie Avenue	UPRR Spur	Railroad Crossing Sign	Crossing to be Decommissioned
Meyer Road	UPRR Spur	Railroad Crossing Sign	Crossing to be Decommissioned

PFHD Map

The PFHD Map was reviewed and updated with current improvements, city boundaries, and annexations. A copy of the PFHD Map is included in Appendix J.

Pavement, Bridge, & Sign Management Strategies

The PFHD utilizes the IWORQ program as part of its pavement management system. The IWORQ program is a global information system (GIS) format map and data system that identifies the roadways, culverts, and signs within the Highway District jurisdiction. Though the Highway District has been entering road, culvert, and sign data, it can utilize the management component of the program even further to assist in developing the Capital Improvement Plan.

The goal of utilizing the IWORQ program is to have available a program that can identify remaining service life and pavement condition information that would be instrumental in programming roadway improvements that meet the goals of the Highway District and the budgets available. The Highway District currently has a regular scheduled program of road maintenance such as crack sealing, chip seals, overlays, and inlays that is scheduled by visual assessment and knowledge of historic road maintenance cycles. The IWORQ program can assist in developing a District-wide scheduling program for anticipating yearly work, whether it is road maintenance, culvert cleaning, or sign inspections, that can meet the growing needs of an expanding Highway District.

The objective of creating and maintaining a reliable Pavement Management System, is to input roadway conditions through regular scheduled pavement condition inspections. It is recommended that roads within the Highway District be inspected at least every three years for pavement conditions to look for signs of wear, fatigue, longitudinal or transverse cracking, patching, and raveling. It is also important to maintain accurate records of roadway improvements. Any improvements should be immediately entered to the program. Annual reports can be run through the program, such as Remaining Service Life, Treatment History, Rating History, with customizable inputs, to develop a yearly review. The IWORQ program can provide systematic recommendations of roads to consider programming into the Capital Improvement Plan based on the data collected and input to the program. The IWORQ program would supplement the knowledge of the Road Supervisor for roads requiring maintenance or full construction. An example of road data sheet with pavement assessment is shown below:

Pavement Information

Date

Pave ID

Road Name

From Address

To Address

Length

Width

Area (Yd^2)

JURISDICTION

LAST YR. SEALED

APPLICATION RATE

SEGCODE

FUNCTIONAL CLASSIFICATION

IMPORTANCE

SPEED LIMIT

FOG SEALED

TYPE OF OIL USED

SURFACE TYPE

Current Rating Information

Date

FATIGUE

LONGITUDINAL

PATCHING

TRANSVERSE

EDGE

BLOCK

Pavement Condition

Date

RSL

Recommended Treatment

Treatment History

Uploaded Files

Date	File	Uploaded By

Figure 8 – IWORQ Pavement Management Data Sheet

The culvert component of the IWORQ program lists the culverts within the Highway District. There are 555 culverts throughout the jurisdiction. The size, material, and conditions of the culverts can be input, with latitude and longitude, and any photos or comments concerning inspection conditions. The IWORQ program will allow a culvert maintenance program to be scheduled to ensure inspections and maintenance are performed on all the culverts in a systemic fashion. The program can break up the request input fields by numerous components, to narrow down a maintenance program specific for the Highway District. A Culvert input page from the IWORQ program is shown below:

Asset Information

ID: Date:

Type:

Quantity/Length:

Quantity/Length:

Road Name:

From Address:

To Address:

CULVERT GPS NUMBER:

DISTANCE FROM SEGMENTS:

Inspection Date:

CULVERT CONDITION:

CULVERT MATERIAL:

COMMENTS:

TYPE OF WELL:

PRE-TREATMENT FACILITIES:

WELL CONSTRUCTION:

Elevation:

Distance:

Latitude:

Longitude:

Maintenance History

Map Info

Latitude

Longitude

Uploaded Files

Date	File	Uploaded By	
12/16/2014 12:45:31 PM	IMG_0637.JPG	Mael, Ed	✗
12/16/2014 12:45:31 PM	IMG_0636.JPG	Mael, Ed	✗
12/16/2014 12:45:31 PM	IMG_0635.JPG	Mael, Ed	✗

Figure 9 – IWORQ Culvert Management Data Sheet

The sign component of the IWORQ program lists the road signs installed within the Highway District. The signs retroreflectivity should be inspected on a regular basis, in order to replace signs reaching minimum retroreflectivity. There are several methods for inspection, with LHTAC recently providing to local jurisdictions retroreflectivity comparison panels to aid in visual inspections. By consistent inspection and replacement, the Highway District can manage their sign inventory through the IWORQ program. An example of the sign data input page from the IWORQ program is shown below:

General Information
History Save Delete

Date:

Sign ID:

MUTCD:

NOTE: SELECT MUTCD looks up signs not in the drop down menu.

Road Name:

From Address:

To Address:

Address:

Position:

Height:

Offset:

District Sign ID #:

SIZE (WIDTH):

SIZE (LENGTH):

SIGN BACKING:

SIGN SHEETING:

ORIENTATION:

SUPPORT SIZE:

SUPPORT MATERIAL:


BASE MATERIAL:

DISTRICT RD CODE:

ACTUAL SIGN NAME:

Visibility Problem:

Speed Limit:



Add Sign:

Current Rating Information

Date:

SIGN CONDITION:

SUPPORT CONDITION:

INVENTORY/REPAIR:

PERSON INSPECTING:

RETIRED:

Sign Condition

Date: 2/17/2017

RSL: 10

Recommended Treatment: NONE

Treatment History

Date	Treatment	Description	
5/1/2011	Yearly Physical Inventory		Edit

Figure 10 – IWORQ Sign Management Data Sheet

Capital Improvement Plan

The PFHD has an existing Capital Improvement Plan that incorporates road improvement projects, intersection improvements, capacity improvements, safety improvements, and maintenance projects.

The Capital Improvement Plan developed through this Transportation Plan was planned, modified, and vetted through a series of technical and community input. It began with the Stakeholder Interviews and proceeded with Technical Advisory Committee discussion concerning areas of concern. Input was gathered from the public at the Open House events and considered when developing the Capital Improvement Plan project list. The Technical Advisory Committee developed and refined the project evaluation criteria, and independently ranked the projects within the Capital Improvement Plan Project Ranking Matrix. The Matrix and Draft Capital Improvement Plan was presented to the public at the second Open House. Comments were gathered concerning the ranking and proposed project scheduling.

Summary of CIP Goals & Objectives

The Capital Improvement Plan provides a five-year forecast of upcoming needs to budget for and pursue potential funding sources. The goals of the Capital Improvement Plan were developed to include the following objectives:

- Safety Improvements
- Transportation Needs and Capacity
- Economic Vitality
- Maintenance Improvements
- Public Support
- Ties to adjacent projects
- Project Costs
- Right-of-Way Needs

In addition, long range planning for projects of greater effort, significance, and funding, such as the potential reconstruction of the Greensferry Bridge, can be tracked and even broken into phases on the five-year Capital Improvement Plan to ensure budget and progress are accounted for.

Recommended CIP Projects

The projects recommended in the 2018 -2022 Capital Improvement Plan is shown below in Figure 11 – Post Falls Highway District Capital Improvements Plan (CIP). The plan identifies the project with a short description, the anticipated year of construction, the type of Capital Improvement (road improvement, intersection improvement, bridge/culvert, safety improvement, bike/pedestrian improvement, or maintenance – bituminous surface treatment), CIP Value, Potential Funding Source, Approved Funding Source, and design year. Below find Figure 12 - PFHD Project Map identifying CIP project locations.



**POST FALLS HIGHWAY DISTRICT
CAPITAL IMPROVEMENTS PLAN
2018-2022
(Draft)**

Anticipated Year of Construction	Project	Type of Capital Improvement	CIP Value	Potential Funding Source	Approved Funding Source	Design Year
2018	Prairie Avenue & Chase Road (roundabout)	Intersection Improvements	\$1,000,000		Strategic Initiative Grant	2017
2018	Huetter Road (Mullan to Prairie, widen shoulders & structural overlay)	Road Improvements	\$400,000		DA, HB312, PFHD	2017
2018	Hauser Lake Road (replace culvert west of Ragged Ridge Road)	Bridges/Culverts	\$30,000		PFHD	2017
2018	Winch Avenue - Church to N. Chase (ADT - 84 Length - 3464')	Bituminous Surface Treatment	\$40,000		PFHD	-
2018	Schilling Loop - Eastside, Riverview to Coyote (ADT - 195, Length - 2709')	Bituminous Surface Treatment	\$32,000		PFHD	-
2019	Riverview Drive (guardrail installation)	Safety Improvements	\$450,000		LHSIP	2017
2019	Riverview Drive at Skalen Creek (widening and guardrails)	Safety Improvements	\$1,000,000		STP Rural	2017
2019	Hauser Lake Road Culvert sizing/elevate roadway)	Road Improvements	\$150,000		HB312 - PFHD	2018
2019	Prairie Avenue (SH-41 to Greensferry, CRABS & overlay)	Road Improvements	\$400,000	HB312 - PFHD	PFHD	2019
2019	Wyoming Avenue (Meyer Rd to Huetter rebuild to gravel standards)	Road Improvements	\$100,000	PFHD		2017
2019	Schilling Loop - Coyote to Comet (ADT - 189 Length - 4179)	Bituminous Surface Treatment	\$50,000		PFHD	-
2020	Spoakne St Bridge Rehab	Bridges/Culverts	\$590,000		STP Bridge	2017
2020	Pleasant View & Prairie Avenue (roundabout)	Intersection Improvements	\$1,200,000	LHSIP	LHSIP	2018
2020	Prairie Avenue & Greensferry Road (signalization)	Intersection Improvements	\$600,000		HB312, PFHD	2019
2020	Seltice Way (Seeley St to Huetter Overlay)	Road Improvements	\$450,000	HB312, PFHD		2020
2020	S. Stateline Rd - End of oil to bottom of hill (ADT - 158 Length 4280')	Bituminous Surface Treatment	\$50,000		PFHD	-
2021	Prairie Avenue & Idaho Street	Intersection Improvements	\$990,000		LHSIP	2020
2021	Riverview Drive (Curve Realignment at St. Dominics)	Road Improvements	\$500,000		DA - PFHD	2021
2021	Riverview Drive (safety improvements)	Safety Improvements	\$450,000	HB312, PFHD		2017
2021	Millsap Loop - Holland Rd to the elk pens (ADT - 104 Length - 3400')	Bituminous Surface Treatment	\$40,000		PFHD	-
2022	Beck Road, Seltice Way to Prairie Avenue (widen shoulders & structural overlay)	Road Improvements	\$2,500,000		STP Rural	2017
2022	Millsap Loop (Big Rock Road rebuild to intersection & hill)	Road Improvements	\$50,000		PFHD	2021
2022	Millsap Loop - Elk pens to Deer Ridge (ADT - 104 Length - 5787')	Bituminous Surface Treatment	\$68,000		PFHD	-
2023	Hayden Avenue & Meyer Road (intersection improvements)	Intersection Improvements	\$1,200,000	STP, PFHD		2019
Preliminary / Not Yet Scheduled	Prairie Avenue (Five Lanes from Meyer to SH-41)	Road Improvements	\$4,200,000	STP/HB312, PFHD		
	Riverview Drive Extension (Foothills to Fairmont Loop)	Road Improvements	NR	Development, STP, HB312, PFHD		
	Upriver Drive (Realignment at Jacobs Loop)	Road Improvements	\$75,000	PFHD		
	Riverview Drive (intersection improvements at Idaho Road)	Intersection Improvements	\$500,000	PFHD		
	Pleasant View & SH-53 (grade separated intersection)	Intersection Improvements	\$2,000,000	Tiger Grant		
	Riverview Drive & Harbor Drive (intersection improvements)	Intersection Improvements	\$150,000	PFHD		
	Huetter Road Bikeway (connect the Prairie Path to the Prairie Avenue bike lanes)	Bike/Pedestrian	\$200,000	Children Pedestrian Safety-TAP Grant		
	Seltice Way Connection to the Centennial Trail (connect the Seltice Shared Use Path to the Centennial Trail)	Bike/Pedestrian	\$50,000	Children Pedestrian Safety-TAP Grant		
	Greensferry Bridge	Bridges/Culverts	\$16,000,000			
Total CIP Infrastructure Improvements			\$35,515,000			

Grant & Funding Sources

Implementation Action List

Successful implementation of this Transportation Plan will require the Capital Improvement Plan to be updated on an annual basis. Potential projects should be re-prioritized with the ranking categories in the CIP Projects Ranking Matrix. Post Falls Highway District should seek funding opportunities yearly to advance projects that meet criteria within grant and funding opportunities. The PFHD should consider the following recommendations in continuing implementation:

Annual Review of Upcoming Grant and Funding Opportunities

The District should review upcoming projects in their CIP to determine the criteria of annual funding opportunities and which projects best fit the requirements. Crash data, estimated costs, right-of-way needs, and capacity criteria should all be reviewed as potential key data that may qualify a project for funding wins. Local agencies provide annual workshops to educate applicants in developing competitive submittals to various funding opportunities. Attendance at these training opportunities are highly suggested in they will typically share key components to successful submittal packages.

T-2 Road Maintenance Training

Maintaining staff that are current in their road maintenance and road safety training will typically yield a higher score on funding applications. LHTAC has a Training and Technical Assistance (T2) program available to Highway Districts for training their staff.

Coordination with Neighboring Jurisdictions and KMPO

Active participation in the KMPO organization and neighboring jurisdictions, provides opportunities to advance projects based on needs of the community and coordination with adjacent projects. KMPO has knowledge and access to project funding opportunities sometimes unique to their organization. Active membership in a multi-jurisdictional transportation group produces higher scores on funding opportunities, as well as project support from the local community. It is recommended PFHD continue their involvement in the Transportation Community and KMPO membership.

Appendices

Appendix A - Stakeholder Questionnaires

Appendix B – Technical Advisory Committee Meeting #1

Appendix C – Open House #1

Appendix D - Technical Advisory Committee Meeting #2

Appendix E – Technical Advisory Meeting #3

Appendix F – Open House #2

Appendix G – Population Data

Appendix H – Urban and Rural Federal Functional Classifications

Appendix I – Inter-Model Maps

Appendix J – Post Falls Highway District Map

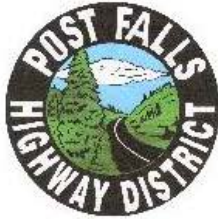
Appendix K – Capital Improvement Plan and CIP Project Map

Appendix A
Stakeholder Questionnaires



POST FALLS HIGHWAY DISTRICT STAKEHOLDER QUESTIONNAIRE SUMMARY OF RESPONSES

Stakeholders Contacted	Response Received	Primary Concerns
<i>Kootenai County Sherriff's Department</i>	X	Congestion, Railroad Crossing Safety
<i>N. Idaho Centennial Trail Foundation</i>		
<i>Kootenai County Airport Manager</i>	X	Congestion, Truck traffic, Airport access
<i>Coeur d' Alene School District</i>	X	Bicycle and Pedestrian Routes
<i>Post Falls School District</i>	X	Bicycle and Pedestrian Routes, Communication with School District during inclement weather
<i>Lakeland School District</i>		
<i>Kootenai County Community Development</i>		
<i>KMPO</i>		
<i>Kootenai Solid Waste</i>		
<i>ITD</i>		
<i>Kootenai County Fire & Rescue</i>	X	Maintenance, Congestion, and Railroad Crossing Safety
<i>Kootenai County Transit</i>	X	More transit facilities
<i>City of Post Falls</i>		
<i>City of Rathdrum</i>		
<i>City of Hayden</i>		
<i>City of Coeur d'Alene</i>	X	Safety, Bicycle & Pedestrian Facilities, developing Complete Streets Concept



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
 POST FALLS, IDAHO 83854
 208-765-3717

Stakeholder Questionnaire

Stakeholder Name:	CHRIS BOSLEY	Date:	3/22/2017
Organization / Position:	CITY OF COEUR D'ALENE / CITY ENGINEER		
Phone:	(208)769-2216	Fax:	(208)769-284
Email:	CBOSLEY@CDAID.ORG		

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
		✓	Safety / Accidents
	✓		Peak hour traffic congestion
	✓		Maintenance
		✓	Bicycle system
		✓	Walkability / Pedestrian systems
			Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
		✓	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or added capacity on roads
<input checked="" type="checkbox"/>	Improved safety
<input checked="" type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input checked="" type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input checked="" type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment

4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

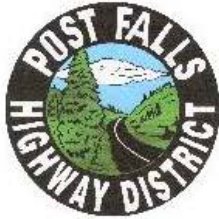
IMPLEMENTING A COMPLETE STREETS APPROACH TO ALL TRANSPORTATION IMPROVEMENTS. FILLING IN GAPS IN THE PEDESTRIAN AND BICYCLE NETWORK. IMPLEMENTING ROUNDABOUTS TO REDUCE CRASH SEVERITY AND REDUCE DELAY DURING OFF-PEAK HOURS.

5. Are there particular areas that need transportation improvements? Please be specific.

Providing bicycle connectivity to the Centennial Trail (through Post Falls) and the SH-41 trail (present and future).

6. Are there particular areas that generate safety concerns? Please be specific.

Prairie Ave appears to have a disproportionately high number of Type A injury and fatality crashes.



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208-765-3717

7. What transportation issues concern you with respect to your community or organization?

THE POTENTIAL TRAFFIC IMPACT OF CONNECTING POLELINE AVE WITH HANLEY AVE. THE NEED TO CREATE A COMPLETE STREETS CORRIDOR WITH SELTICE WAY ONCE THE COEUR D'ALENE PORTION IS COMPLETED.

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

IMPROVEMENT OR ELIMINATION OF AT-GRADE RAILROAD CROSSINGS.

9. Do you have any ideas for goals & objectives for the Transportation Plan?

INCREASED CONNECTIVITY FOR PEDESTRIAN AND BICYCLE TRAVEL.

10. What are your impressions of the bicycle & pedestrian facilities within the PFHD jurisdiction?

MORE ON-STREET BIKE LANES, SHARED-USE PATHS, AND SIDEWALKS ARE NEEDED AND WOULD GREATLY IMPROVE CONNECTIVITY, ALLOWING THE COMMUNITY TO SHIFT MODE SHARE TOWARD NON-MOTORIZED TRANSPORTATION AND IMPROVING LIVABILITY FOR THE COMMUNITY.

11. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

BICYCLE FACILITIES SHOULD BE FOCUSED AROUND PROVIDING CONNECTIONS TO THE CENTENNIAL TRAIL AND FUTURE SH-41 TRAIL (THE BACKBONE OF THE SYSTEM). SHARED USE PATHS AND BIKE LANES SHOULD ALSO PROVIDE ACCESS TO SCHOOLS AND PARKS. SIDEWALKS SHOULD BE INSTALLED WHERE GAPS IN THE EXISTING SIDEWALK SYSTEM EXIST.

12. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

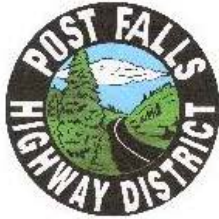
Yes No

13. What areas within the PFHD jurisdiction do you think are most likely to develop in the next 20 years?

THE AREA BETWEEN POST FALLS AND COEUR D'ALENE

14. Do you believe that new development and / or redevelopment activities will generate significant traffic congestion and parking problems within the PFHD jurisdiction? If so, where do you think these problem areas will be and what do you believe are potential solutions?

No



POST FALLS HIGHWAY DISTRICT
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 208-765-3717

Stakeholder Questionnaire

Stakeholder Name: PHILLIP CUMMINGS		Date: 4-10-17
Organization / Position: COEUR D'ALENE AIRPORT \ OPERATION MGR.		
Phone: 208-446-1860	Fax: 208-446-1867	Email: PCUMMINGS@KCGOV.US

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	✓		Safety / Accidents
	✓		Peak hour traffic congestion
	✓		Maintenance
	✓		Bicycle system
	✓		Walkability / Pedestrian systems
	✓		Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
		✓	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

✓	Maintenance / Repair of existing transportation infrastructure
✓	New roads or added capacity on roads
	Improved safety
	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
✓	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

	Reducing fatalities / injuries
	Maintaining current infrastructure
✓	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment

4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

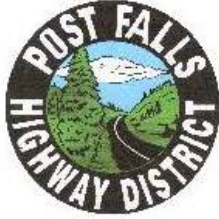
CAPACITY AND TRUCKING NETWORK, THE INCREASE IN ALL TRAFFIC ON THE CURRENT SYSTEM WHEN TAKING INTO VIEW THE PLANNED DEVELOPMENT UNDER WAY COMMERCIAL ACCESS HAS NO CHOICE BUT TO MIX WITH LOCAL (RESIDENTIAL) TRAFFIC.

5. Are there particular areas that need transportation improvements? Please be specific.

pleasant view and highway 41 feed main north south, Hayden ave from McGuire rd. to atlas rd. and Lancaster from 41 to Huetter rd., I believe need some trucking consideration.

6. Are there particular areas that generate safety concerns? Please be specific.

Pleasant view at prairie and hwy 53. Lancaster at hwy 41.



POST FALLS HIGHWAY DISTRICT
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7. What transportation issues concern you with respect to your community or organization?

EAST WEST ACCESS TO AIRPORT AND COMMERCIAL \ INDUSTRIAL DEVELOPMENT.

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

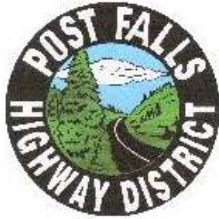
REGIONAL GROWTH COMMERCIAL \ INDUSTRIAL AND RESIDENTIAL, THE HIGHWAY DISTRICTS AND AIRPORT NEED TO PLAN TOGETHER FOR A MOBILE FUTURE TRANSPORTING MORE PEOPLE AND GOODS WITH BETTER ACCESS.

9. Do you have any ideas for goals & objectives for the Transportation Plan?

WE HOPE TO UNDERSTAND THE NORTH ,SOUTH AND EAST ,WEST FUTURE ACCESS PLANS FOR THE AIRPORT.

10. What route improvements would be of greatest value to your organization?

HAYDEN AVE. EAST TO ATLAS RD. AND LANCASTER EAST TO HUETTER RD.



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
 POST FALLS, IDAHO 83854
 208-765-3717

Stakeholder Questionnaire

Stakeholder Name: BRIAN WALLACE		Date: 4/7/17
Organization / Position: CDA SCHOOL DISTRICT #271		
Phone: 208-664-8241	Fax: 208-664-1748	Email: BWALLACE@CDASCHOOLS.ORG

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input checked="" type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or added capacity on roads
<input checked="" type="checkbox"/>	Improved safety
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input checked="" type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input checked="" type="checkbox"/>	Reducing fatalities / injuries
<input checked="" type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment

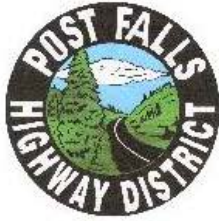
4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

SAFE WALKING AND BIKE ROUTES, WELL MAINTAINED ROADS, TRAFFIC CONGESTION KEPT TO A MINIMUM.

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

Trees and shrubs at intersections can cause difficulty seeing.



POST FALLS HIGHWAY DISTRICT
5629 EAST SELTICE WAY
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7. What transportation issues concern you with respect to your community or organization?

FROM A SCHOOL DISTRICT PERSPECTIVE, IT IS VERY IMPORTANT TO HAVE SAFE WALKING/BIKING ROUTES TO SCHOOLS AND SAFE TRAFFIC ZONES AROUND SCHOOLS

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

9. Do you have any ideas for goals & objectives for the Transportation Plan?

10. What portion(s) within the PFHD jurisdiction is difficult to access? Please be specific.

11. Do you think better road signage is needed? If so, where?

12. What route improvements would be of greatest value to your organization?

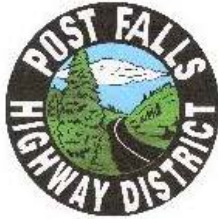
13. What are your impressions of the bicycle & pedestrian facilities within the PFHD jurisdiction?

14. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

FAMILIES NEED SAFE OPTIONS AND CONNECTIVITY FOR THEIR CHILDREN TO GET TO AND FROM THEIR NEIGHBORHOOD SCHOOLS AND PARKS.

15. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
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 208-765-3717

Stakeholder Questionnaire

Stakeholder Name:	WARREN MERRITT	Date:	3-21-17
Organization / Position:	KOOTENAI COUNTY FIRE & RESCUE		
Phone:	208-777-8500	Fax:	208-777-1569
Email:	WARREN@KOOTENAIFIRE.COM		

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good Fair Poor

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input checked="" type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or added capacity on roads
<input checked="" type="checkbox"/>	Improved safety
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input checked="" type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input type="checkbox"/>	Reducing fatalities / injuries
<input checked="" type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment

4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

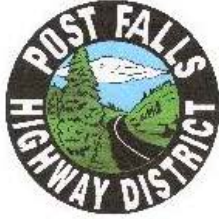
AT BUSY INTERSECTIONS HAVING ADEQUATE SIGNALING AND PEDESTRIAN ACCOMMODATIONS AVAILABLE

5. Are there particular areas that need transportation improvements? Please be specific.

Cougar Gulch area, Maintenance of Seltice Way,

6. Are there particular areas that generate safety concerns? Please be specific.

Railroad Crossings; realize that the PFHD can't always fix these but more signals if there is an opportunity to impact the same is important.



POST FALLS HIGHWAY DISTRICT
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208-765-3717

7. What transportation issues concern you with respect to your community or organization?

MAINTENANCE OF ROADS DO TO OUR HEAVY FIRE APPARATUS

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

1. APPROPRIATE CURBING IN THE CONGESTED AREAS WHERE PEDESTRIANS ARE
2. SIGNALIZATION BASED ON NEED

9. Do you have any ideas for goals & objectives for the Transportation Plan?

1. OVERLAY PROGRAM ON ROADS WITH HIGH VOLUMES
2. CHIP SEAL ON LESS TRAVELED STREETS

10. What portion(s) within the PFHD jurisdiction is difficult to access? Please be specific.

1. NEED TO BE CAREFUL ALONG SELTICE WAY BETWEEN CEDAR AND ATLAS

11. Do you think better road signage is needed? If so, where?

PROBABLY OK

12. What route improvements would be of greatest value to your organization?

SELTICE WA, PRAIRIE AVE, PLEASANTVIEW (RAILROAD TRACKS)

13. What are your impressions of the bicycle & pedestrian facilities within the PFHD jurisdiction?

I DON'T RIDE A BIKE AND THERE ARE AREAS WHERE BIKES DON'T BELONG - LACK OF A SHOULDER; RIVERVIEW BEING ONE OF THEM.

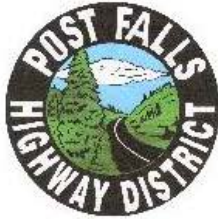
14. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

ON STREETS THAT WOULD ACCESS THE CENTENNIAL TRAIL SYSTEM

I SAY YES BUT ADD; NOT EVERY ROAD NEEDS BICYCLE ACCOMMODATION

15. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
 POST FALLS, IDAHO 83854
 208-765-3717

Stakeholder Questionnaire

Stakeholder Name:	JOE JOVICK		Date:	03/27/17	
Organization / Position:	KOOTENAI COUNTY SHERIFF'S OFFICE, PATROL LIEUTENANT				
Phone:	208-446-2245	Fax:	208-446-1307	Email:	JJOVICK@KCGOV.US

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	✓		Safety / Accidents
	✓		Peak hour traffic congestion
✓			Maintenance
	✓		Bicycle system
	✓		Walkability / Pedestrian systems
	✓		Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
	✓		Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

✓	Maintenance / Repair of existing transportation infrastructure
✓	New roads or added capacity on roads
	Improved safety
✓	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

	Reducing fatalities / injuries
	Maintaining current infrastructure
✓	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment

4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

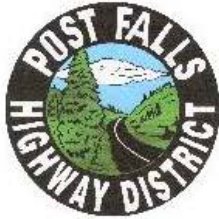
IMPROVEMENTS AT RAILROAD CROSSING, TO INCLUDE A BYPASS NEAR HWY 53 AND STATELINE, MUCH LIKE THE ONE NEAR GREENSFERRY AND HWY 53. WIDENING OF PRAIRIE AVENUE FROM MEYER ROAD TO STATE LINE AND DEVELOPMENT OF FRONTAGE ROADWAYS ALONG HIGHWAY 41.

5. Are there particular areas that need transportation improvements? Please be specific.

Prairie Avenue between Meyer Road and Highway 41, with improvements at the Hwy 41 and Prairie Avenue interchange.

6. Are there particular areas that generate safety concerns? Please be specific.

Although I believe most of Lancaster Road between Greensferry and Huetter Road is now in the City of Rathdrum, with 2 schools now on Lancaster and multiple housing developments occurring, this area is becoming a high traffic area and improvements are going to be needed. Meyer Road needs improved access at Highway 53 and possible improved traffic control devices to Hayden Avenue.



POST FALLS HIGHWAY DISTRICT
5629 EAST SELTICE WAY
POST FALLS, IDAHO 83854
208-765-3717

7. What transportation issues concern you with respect to your community or organization?

NUMEROUS CRASHES ALONG HAYDEN, MEYER, HUETTER AND GREENSFERRY ROADS. ALSO TRAFFIC HAZARDS ALONG BECK ROAD DURING EVENTS AT STATE LINE RACEWAY. ROADWAY NEEDS TO BE WIDENED AND A BETTER SHOULDER OR WALKING PATH NEEDS TO BE ESTABLISHED FOR PEDESTRIAN TRAFFIC IN THE AREA.

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

CONTINUED SPRAWL ACROSS THE PRAIRIE, WHICH WILL MAKE IT HARDER TO OBTAIN THE LANDS NEEDED TO CREATE IMPROVEMENTS, BY WIDENING OF ROADWAYS OR BUILDING OF TURN LANES OR TRAFFIC CIRCLES.

9. Do you have any ideas for goals & objectives for the Transportation Plan?

WITH INCREASED TRAIN TAFFIC ON BOTH BNSF AND UNION PACIFIC RAIL LINES, UNDERPASSES OR BRIDGES TO ASSIST WITH TRAFFIC FLOW IS NEEDED.

10. What portion(s) within the PFHD jurisdiction is difficult to access? Please be specific.

AT TIMES, TRAFFIC BECOMES SEVERELY BACKED UP AT PRAIRIE AVENUE AND HWY 53, ALONG WITH HEAVY TRAFFIC ALONG PRAIRIE AVENUE BETWEEN MEYER AND BECK, WHICH FOR THE MOST PART IS ALL STILL ONLY 2 LANES.

11. Do you think better road signage is needed? If so, where?

N/A

12. What route improvements would be of greatest value to your organization?

IT WAS GREAT SEEING LIGHTS PUT IN AT HUETTER AND SELTICE WAY AND THE IMPROVEMENTS THAT HAVE BEEN MADE ALONG PRAIRE TO MEYER ROAD, BUT WITH CONTINUE GROWTH IN THE GREATER RATHDRUM AREA, CONTINUED IMPROVEMENTS, TO INCLUDE RAILROAD UNDER/OVER PASSES ARE NEEDED.

13. What are your impressions of the bicycle & pedestrian facilities within the PFHD jurisdiction?

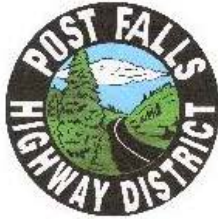
N/A

14. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

ALONG ABANDON RAILWAYS OR OTHER TRAIL SYSTEMS. I UNDERSTAND THERE SEEMS TO BE A BIG PUSH FOR "SHARING THE ROADWAYS" BUT LAW OF THE LUG NUTS SAYS THOSE WITH THE MOST LUG NUTS WINS, WHICH MEANS BICYCLISTS ALWAYS LOOSE IN VEHICLE VS. BICYCLIST CRASHES AND I BELIEVE IT SAFER FOR THEM TO BE RIDING IN CITIES OR ON TRAILS, AND NOT ALONG RURAL ROADS WITH HIGHER SPEEDS.

15. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
 POST FALLS, IDAHO 83854
 208-765-3717

Stakeholder Questionnaire

Stakeholder Name:	KOOTENAI COUNTY TRANSIT		Date:	4/5/2017	
Organization / Position:	PROGRAM SPECIALIST				
Phone:	208-446-1616	Fax:	208-446-1039	Email:	KHOBSON@KCGOV.US

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good Fair Poor

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Peak hour traffic congestion
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Transit system

I can't answer this as I would really like to expand transit in the Post Falls area.

2. Which 3 issues are most important to address in the Transportation Plan?

<input checked="" type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input checked="" type="checkbox"/>	New roads or added capacity on roads
<input type="checkbox"/>	Improved safety
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

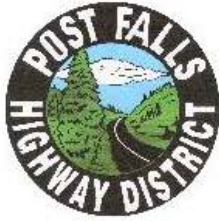
<input type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
<input checked="" type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment

4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

MORE INVESTMENT INTO PUBLIC TRANSPORTATION IN THE AREA

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.



POST FALLS HIGHWAY DISTRICT
5629 EAST SELTICE WAY
POST FALLS, IDAHO 83854
208-765-3717

7. What transportation issues concern you with respect to your community or organization?

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

9. Do you have any ideas for goals & objectives for the Transportation Plan?

10. Do you think better road signage is needed? If so, where?

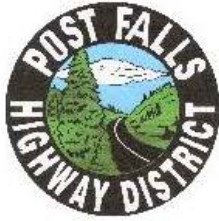
11. What route improvements would be of greatest value to your organization?

12. What are your impressions of the bicycle & pedestrian facilities within the PFHD jurisdiction?

13. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

14. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No



POST FALLS HIGHWAY DISTRICT
5629 EAST SELTICE WAY
POST FALLS, IDAHO 83854
208-765-3717

Stakeholder Questionnaire

Stakeholder Name: POST FALLS SCHOOL DISTRICT		Date: APRIL 7, 2017	
Organization / Position: SUPERINTENDENT			
Phone: 773-1658	Fax: 773-3218	Email: JKEANE@SD273.COM	

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good Fair Poor

	Good	Fair	Poor	
<input checked="" type="checkbox"/>				Safety / Accidents
<input checked="" type="checkbox"/>				Peak hour traffic congestion
<input checked="" type="checkbox"/>				Maintenance
		<input checked="" type="checkbox"/>		Bicycle system
			<input checked="" type="checkbox"/>	Walkability / Pedestrian systems
<input checked="" type="checkbox"/>				Truck traffic
<input checked="" type="checkbox"/>				Traffic operations (i.e. traffic signal timing / coordination)
<input checked="" type="checkbox"/>				Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input checked="" type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or added capacity on roads
<input type="checkbox"/>	Improved safety
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input checked="" type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input checked="" type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input type="checkbox"/>	Reducing fatalities / injuries
<input checked="" type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment

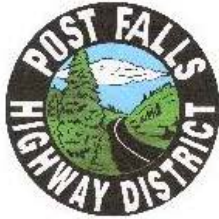
4. What types of transportation improvements do you see as being most beneficial to the region's quality of life?

ADDITIONAL BICYCLE AND PEDESTRIAN ROUTES

5. Are there particular areas that need transportation improvements? Please be specific.

It would be helpful that the hired officials would provide the district with more information regarding the timing and issues they are facing when their is inclement weather.

6. Are there particular areas that generate safety concerns? Please be specific.



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7. What transportation issues concern you with respect to your community or organization?

OVERALL WE DO NOT HAVE CONCERNS. HOWEVER SEE ABOVE. THE COMMUNICATION WITH US IS VERY RESTRICTED.

8. What other issues / factors do we need to take into consideration in the Transportation Plan?

9. Do you have any ideas for goals & objectives for the Transportation Plan?

10. What portion(s) within the PFHD jurisdiction is difficult to access? Please be specific.

11. Do you think better road signage is needed? If so, where?

SIGNAGE IS GOOD.

12. What route improvements would be of greatest value to your organization?

13. What are your impressions of the bicycle & pedestrian facilities within the PFHD jurisdiction?

THEY ARE LIMITED.

14. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

CLOSE TO VARIOUS CONCENTRATION OF HOMES.

15. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

Appendix B

Technical Advisory Committee Meeting #1



**POST FALLS HIGHWAY DISTRICT
TECHNICAL ADVISORY COMMITTEE
MEETING #1
SUMMARY OF DISCUSSION**

August 17, 2017

Safety Concerns

Highway 53 intersections
Railroad Crossings
Bus Stops
Riverview Drive - Bike Safety
Lancaster & Huetter Intersection
Lancaster & Meyer Intersection
Lancaster & SH-41 Intersection
Huetter Bike Facility
Bike/ped on Seltice
Huetter Bypass
Pleasant View & Prairie Intersection
Bike Connectivity throughout the District

Areas with Active or Expected Growth or Change

Lancaster - 70 + 90 lots (could see another 100)
Pleasant View Road & Beck Road
Atlas/Huetter/Lancaster
Hanley/Poleline @ Huetter
Prairie Avenue / SH-41
SH-41 Corridor, especially Horsehaven

Strengths & Areas for Improvement

Strengths

- Participation in Regional Growth & Collaboration Efforts
- Active solving of Safety Concerns within the district

Areas for Improvement

Increase communication with residents
Send out mailers for areas with upcoming construction
KMPO has developed a website to update regional construction the District could link to
Media Outreach - Facebook, Twitter

Proposed Future Projects

Greensferry Bridge

Appendix C
Open House #1



Post Falls Highway District Transportation Plan Open House

Post Falls Highway District is seeking public input on their Transportation Plan.

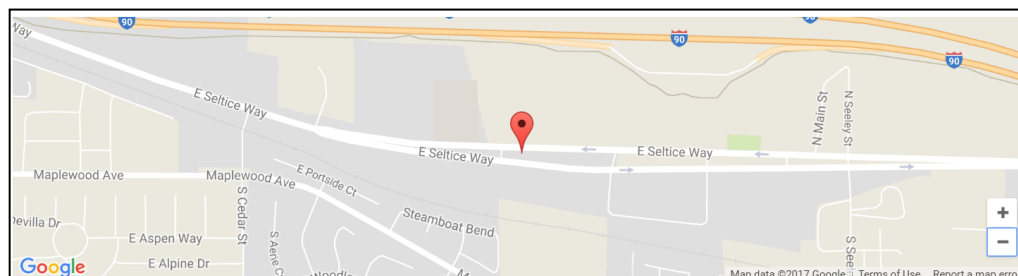
Please join us

Tuesday, September 26, 2017

4:00 pm to 7:00 pm

Post Falls Highway District Office

5629 E. Seltice Way





Post Falls Highway District Summary of Public Open House #1 Questionnaire

9/26/2017

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
10	10	1	Safety / Accidents
5	12	5	Peak hour traffic congestion
12	8	2	Maintenance
2	11	9	Bicycle system
3	9	11	Walkability / Pedestrian systems
7	13	2	Truck traffic
9	10	2	Traffic operations (i.e. traffic signal timing / coordination)
3	5	8	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

17	Maintenance / Repair of existing transportation infrastructure
14	New roads or added capacity on roads
13	Improved safety
12	Improved road operations & traffic signal timing
1	Improved road signage
8	Additional bicycle & pedestrian Facilities
4	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

13	Reducing fatalities / injuries
8	Maintaining current infrastructure
11	Reducing congestion
9	Improving roadway reliability
1	Improving the trucking network
2	Protecting / enhancing the environment

One citizen wrote in support for multi-modal facilities

4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes

17

No

2

Yes, only if bicyclists are required to use shoulders

5. Are there particular areas that need transportation improvements? Please be specific.

- Beck Road is really narrow and you cannot pull off on the side of the road
- The higher traffic/main roads need more access to Hwy 53 from Hayden Ave
- Put in bridges either over or under railroad intersections
- Pleasant View
- Highway 41 & Prairie, Mullan, & Seltice
- 4th Ave/Seltice – needs a light or roundabout
- Prairie Ave widening between Greensferry & Meyer
- Intersection Improvements along Prairie
- Greensferry River Bridge
- Hwy 41 – Post Falls & Rathdrum
- Gunning in Rathdrum needs turn lane to Gunning, too many accidents happen there
- Get traffic off of Prairie, crash #'s are just a direct result of volume
- Signal timing, often it feels like the system is set up to stop people, rather than keep them moving. Very noticeable in situations of low traffic volumes.
- Hwy 41 – 4 lanes to Rathdrum
- Signal at Lancaster/Hwy 41
- Pleasant View & 53; Prairie & Pleasant View; surface condition between Chase & Idaho on Prairie
- Spokane St and Prairie
- Improve Beck Road with heavy truck traffic in mind
- Traffic lights on Greensferry, Chase, Pleasant View, and no roundabouts
- Hwy 53 from State Line to McGuire, including Hauser junction area and Hauser Lake Rd, Beck/Prairie, Pleasant View
- We need an exit ramp to Huetter Road
- The infrastructure for the continued growth of the city
- Schilling Loop – needs paved, several areas along creek are deteriorating and dangerous, Centennial grading necessary with increased traffic. This is a growth area

6. Are there particular areas that generate safety concerns? Please be specific.

- Poleline between Seltice & Hayden
- Prairie between Seltice & Hayden
- Railroad Crossings
- Intersections where truck traffic and passenger traffic meet
- Pleasant View/53
- Highway 41 & Prairie, Mullan, & Seltice
- 4th Ave/Seltice – needs a light or roundabout
- Prairie Avenue
- Pleasant View – Poleline = stop light = not roundabout
- Chase = Prairie = stop light not roundabout
- The potential closure of the SH-53/Prairie & SH-53/McGuire RR crossings is a bad idea. The volume of funneling this would create is dramatic. Create an underpass at McGuire as well as extending Hayden to 53 & add an additional underpass. Create more flow to routes besides Prairie.
- Even though not identified as a Top 10 Crash Location, SH-53 and Pleasant View is a nightmare. I avoid it
- Prairie Avenue
- Prairie & Pleasant View
- Hauser Lake Road pedestrians / speed
- Pleasant View at Prairie and at Hwy 53
- Prairie Ave use traffic signal
- Hwy 53 from State Line to McGuire, including Hauser junction area and Hauser Lake Rd, Beck/Prairie, Pleasant View, Lancaster & 41
- Prairie Ave
- Corner of Schilling Loop (w) and W Riverview – incoming traffic from Riverview will generally cut short and it would be good to have some form of channeling to keep traffic in lane

7. What transportation issues concern you with respect to your community?

- Proper & regular maintenance
- Designs need to address motorist/pedestrian safety & reducing accidents
- Transit
- Bike/Ped
- Maintenance
- Traffic congestion at the intersections of Hwy41 & Prairie, Mullan, & Seltice, and 4th Ave/Seltice
- Growing Congestion at major intersections reducing safety and impacting system efficiency
- Lack of sufficient shoulders for bikes and safe vehicle recovery of errant vehicles
- Need access roads to take away from congestion on Hwy 41 from Post Falls to Rathdrum
- As populations increase, traffic congestion increases with it. There needs to be some focus towards the development and increased opportunity for alternate means of travel. Encourage people to walk & bike & provide the infrastructure for that support.
- Would like the Greensferry Bridge! Need to create an alternative south of river other than Spokane St.
- Timing of lights
- Adequate budgeting for growing construction cost
- Encourage roundabouts at arterial intersections
- Add multi-modal facilities on arterials and collectors
- Prepare for future growth – streets and bike paths
- Turning onto Hwy 53 from Hauser Lake Road
- Providing “connectivity” in the I-90 corridor, i.e. access over/under I-90 in key locations, for example Treaty Rock, Seltice Crossing, @ Hwy 41, Spokane St
- Safety & Growth
- Continued grading and proper snow removal
- Lack of promotion to grow & use pedestrian/cyclist “trails” and/or shoulders connecting vital locations

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

- South side of Spokane River to Greensferry area on north side of Spokane River
- The Riverview/South River area, with limited ways to cross the river
- Prairie/SH-41
- Prairie/Idaho
- Prairie/Spokane
- SH-41 & 16th needs a light for traffic – not a roundabout
- South of the river. The bridge project at Greensferry & Pleasant View would result in great strides for emergency response as well as funneled traffic issues at Spokane St.
- Access more choices south of Spokane River, Greensferry Bridge, someday Pleasant View Bridge
- South Greensferry
- Properties south of Spokane river have limited access. Adding Greensferry Bridge would improve safety and reduce congestion on Spokane St
- Bridge at Pleasant View may be worth considering in long term plan
- None – just plan for future growth
- Hwy 41 between Mullan and Seltice is congested
- Rathdrum trying to cross back and forth over the tracks

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

- Along the main/higher traffic roadways
- Prairie, Hayden, Huetter, Riverview, & include bike/ped with Pleasant View/53 Crossing
- Prairie Avenue
- Seltice Way
- SH-41, Prairie Ave, Poleline Avenue, Greensferry, Riverview Drive
- Automobile traffic is more important and safety standards. A lot of people are not walking or riding bikes in the winter months
- Along identified commuter routes connecting neighborhoods to communities/transit facilities
- Along Hwy 41, along Seltice Way
- Prairie
- Prairie
- Collectors and up
- Near all schools
- Better connectivity around I-90 – local paths along rail easements
- More city cycling & walking between parks. Don't need as many facilities for st mile cyclists, kids & strollers
- Hard to cross Seltice in the Chase-Idaho section
- Seltice, broken sidewalks
- I wouldn't. This is creating a very dangerous situation.



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
 POST FALLS, IDAHO 83854
 208-765-3717

Public Involvement Questionnaire

Name: Bob Nonini	Email: nonini.bob@gmail.com
Address: 5875 W. Harbor Drive, Cda., ID 83814	
Phone Number: 208-659-4643	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
X			Safety / Accidents
	X		Peak hour traffic congestion
X			Maintenance
		X	Bicycle system
		X	Walkability / Pedestrian systems
	X		Truck traffic
X			Traffic operations (i.e. traffic signal timing / coordination)
			Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

1	Maintenance / Repair of existing transportation infrastructure
2	New roads or added capacity on roads
3	Improved safety
	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

	Reducing fatalities / injuries
X	Maintaining current infrastructure
	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



POST FALLS HIGHWAY DISTRICT
5629 EAST SELTICE WAY
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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

^{east west}
Poleline north/south between Seltice
Way and Hayden Ave
Prairie between the same cross roads

7. What transportation issues concern you with respect to your community?

proper & regular maintenance

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

from south side of Spokane River
to Greenferry area on north side
of Spokane River

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



POST FALLS HIGHWAY DISTRICT
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Public Involvement Questionnaire

Name: <u>Nathan Tull</u>	Email: <u>nattull@yahoo.com</u>
Address: <u>4069 W Pleasant Ln Post Falls</u>	
Phone Number: <u>208-264-6287</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<u>1</u>	Maintenance / Repair of existing transportation infrastructure
<u>3</u>	New roads or added capacity on roads
	Improved safety
<u>2</u>	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<u>3</u>	Reducing fatalities / injuries
<u>1</u>	Maintaining current infrastructure
<u>2</u>	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



POST FALLS HIGHWAY DISTRICT
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208-765-3717

4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Beck Rd really narrow road
cant pull off on the side of the road

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



POST FALLS HIGHWAY DISTRICT
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Public Involvement Questionnaire

Name: <i>Heather Carroll</i>	Email: <i>heather.v.carroll@hcdinc.com</i>
Address: <i>954 N 16th St., LDA, ID 83814</i>	
Phone Number: <i>208-890-3740</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



POST FALLS HIGHWAY DISTRICT
5629 EAST SELTICE WAY
POST FALLS, IDAHO 83854
208-765-3717

4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

Truail road crossings

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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 208-765-3717

Public Involvement Questionnaire

Name: <i>LT. MIKE MCFARLAND</i>	Email: <i>MSMCFARLAND@KCGOV.US</i>
Address: <i>5500 N. SPITT. WAY/PO Box 9008, CDA, ID. 83816</i>	
Phone Number: <i>208/446-1300</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	✓		Safety / Accidents
	✓		Peak hour traffic congestion
	✓		Maintenance
		✓	Bicycle system
	✓		Walkability / Pedestrian systems
	✓		Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
		✓	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

THE HIGHER TRAFFIC/MAIN ROADS
MORE ACCESS TO HAYDEN HWY 53 FROM HAYDEN AVE
PUT IN BRIDGES EITHER OVER OR UNDER
RAILROAD INTERSECTIONS -

6. Are there particular areas that generate safety concerns? Please be specific.

INTERSECTIONS WHERE TRUCK TRAFFIC &
PASSENGER TRAFFIC MEET

7. What transportation issues concern you with respect to your community?

DESIGNS NEED TO ADDRESS MOTORIST/PED.
SAFETY & REDUCING ACCIDENTS

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

THE RIVERVIEW/S. RIVER AREA, WITH
LIMIT WAYS TO CROSS THE RIVER

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

ALONG TO MAIN/HIGHER TRAFFIC
ROADWAYS.



POST FALLS HIGHWAY DISTRICT
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 208-765-3717

Public Involvement Questionnaire

Name: <i>Bill Melun</i>	Email: <i>BMELUN@POSTFALLSFOAM.ORG</i>
Address: <i>400 Spokane St Post Falls ID 83854</i>	
Phone Number: <i>208 454-3329</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	<i>/</i>		Safety / Accidents
<i>/</i>			Peak hour traffic congestion
<i>/</i>			Maintenance
	<i>/</i>		Bicycle system
	<i>/</i>		Walkability / Pedestrian systems
<i>/</i>			Truck traffic
<i>/</i>			Traffic operations (i.e. traffic signal timing / coordination)
	<i>/</i>		Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Pleasantville

6. Are there particular areas that generate safety concerns? Please be specific.

- ~~Highway~~ Pleasantville / 53

7. What transportation issues concern you with respect to your community?

- TRANSIT
- BIKE / PED
- MAINTENANCE

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

PRAIRIE, HAYDEN, HUETER, RINGVILLE
INCLUDE BIKE / PED AT PLEASANTVILLE / 53
CROSSING



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 208-765-3717

6

Public Involvement Questionnaire

Name: <u>Diane Fountain</u>	Email:
Address: <u>4218 N. Ceres St CDA</u>	
Phone Number: <u>208-651-4723</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	X		Safety / Accidents
		X	Peak hour traffic congestion - <u>Prairie Ave</u>
X			Maintenance
	X		Bicycle system
	X		Walkability / Pedestrian systems
X			Truck traffic
X			Traffic operations (i.e. traffic signal timing / coordination) <u>except on Hwy 41</u>
X			Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
X	New roads or added capacity on roads
X	Improved safety
X	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
X	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment



POST FALLS HIGHWAY DISTRICT
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208-765-3717

4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Highway 41 → Prairie Ave
Highway 41 → Mullan
✓ ✓ & Seltice
4th Ave / Seltice — Needs a light or round-a-bout

6. Are there particular areas that generate safety concerns? Please be specific.

The above intersections

7. What transportation issues concern you with respect to your community?

Traffic congestion @ the intersections in #5.

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

Prairie Ave
Seltice Way



POST FALLS HIGHWAY DISTRICT
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 208-765-3717

Public Involvement Questionnaire

Name: <i>Robert Palus</i>	Email: <i>RPALUS@PostFalls</i>
Address: <i>408 N. SPOKANE ST. Post Falls ID</i>	
Phone Number:	Check box if you would like a response to your comments <input type="checkbox"/>

*idaho. idg
83854*

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good Fair Poor

Good	Fair	Poor	
			Safety / Accidents
	X		Peak hour traffic congestion
X			Maintenance
		X	Bicycle system
		X	Walkability / Pedestrian systems
X			Truck traffic
	X		Traffic operations (i.e. traffic signal timing / coordination)
		X	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

* Prairie Ave widening between Greensferry and Meyer
* Intersection safety improvements along Prairie Ave
* Greensferry River bridge

6. Are there particular areas that generate safety concerns? Please be specific.

Prairie Ave

7. What transportation issues concern you with respect to your community?

* growing congestion at major intersections reducing safety and impacting system efficiency
* lack of sufficient shoulders for bikes and safe vehicle recovery of errant vehicles

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

Prairie / SH 41 Prairie / Idaho Prairie / Spokane

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

SH 41, Prairie Ave, Poleline Ave., Greensferry, Riverview Dr.



POST FALLS HIGHWAY DISTRICT
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 208-765-3717

Public Involvement Questionnaire

Name: <i>Scott & Pam Borek</i>	Email: <i>P.borek@yahoo.com</i>
Address: <i>P.O. 1166 Post Falls, Idaho</i>	
Phone Number: <i>208 773 0337</i>	Check box if you would like a response to your comments <input checked="" type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
		X	Safety / Accidents
		X	Peak hour traffic congestion
	X		Maintenance
	X		Bicycle system
	X		Walkability / Pedestrian systems
	X		Truck traffic
		X	Traffic operations (i.e. traffic signal timing / coordination)
	X		Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

X	Maintenance / Repair of existing transportation infrastructure
X	New roads or added capacity on roads
	Improved safety
X	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

X	Reducing fatalities / injuries
	Maintaining current infrastructure
	Reducing congestion
X	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

41 Hwy - PF - Rathdrum
Gunning In Rathdrum Needs Turn lane to Gunning. To
Many Accidents happening there

6. Are there particular areas that generate safety concerns? Please be specific.

Pleasant View - Pole Line = Stop Light = Not round about
Chase = Priority = Stop Light Not round about

7. What transportation issues concern you with respect to your community?

Needs Access Roads to take away from
Congestion on Hwy 41 from PF to Rathdrum

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

16th - 41 Hwy needs a light for traffic - not a round about

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

Automobile traffic is more important and safety
standards. A lot of people are not walking & riding bikes
in the winter months



POST FALLS HIGHWAY DISTRICT
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Public Involvement Questionnaire

Name: SGT. WARD CRAWFORD, KCSO	Email: wrcrawford@kegov.us
Address: KCSO	
Phone Number: 208.446.1300	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	✓		Safety / Accidents
✓			Peak hour traffic congestion
	✓		Maintenance
	✓		Bicycle system
✓			Walkability / Pedestrian systems
✓			Truck traffic
✓			Traffic operations (i.e. traffic signal timing / coordination)
	✓		Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

2	Maintenance / Repair of existing transportation infrastructure
3	New roads or added capacity on roads
1	Improved safety
	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

1	Reducing fatalities / injuries
2	Maintaining current infrastructure
	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
3	Protecting / enhancing the environment



POST FALLS HIGHWAY DISTRICT
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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Get traffic off Prairie.
Crash #'s are just a direct result of
volume.

6. Are there particular areas that generate safety concerns? Please be specific.

The potential closure of the 53/~~Prairie~~ & 53/McGuire
RR crossings is a bad idea. The volume of funneling
this would create is dramatic. Create an underpass
at McGuire as well as extending Hayden to 53
& add an additional underpass. Create more flow to routes besides
Prairie!

7. What transportation issues concern you with respect to your community?

As populations increase traffic congestion increases with it.
There needs to be some focus towards the development
and increased opportunity for alternate means of travel.
Encourage people to walk & bike & provide the infrastructure
for that support.

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

South of the river. The bridge projects @ Greenstony
& pleasant view would result in great strides for
emergency response as well as funneled traffic
issues @ Spokane St.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



POST FALLS HIGHWAY DISTRICT
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 208-765-3717

Public Involvement Questionnaire

Name: <u>Jon Manley</u>	Email: <u>jmanley@postfallsidaho.org</u>
Address: <u>400 N. Spokane St</u>	
Phone Number: <u>208-512-0416</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	X		Safety / Accidents
		X	Peak hour traffic congestion
	X		Maintenance
		X	Bicycle system
		X	Walkability / Pedestrian systems
X			Truck traffic
	X		Traffic operations (i.e. traffic signal timing / coordination)
		X	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or added capacity on roads
X	Improved safety
X	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
X	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

1	Reducing fatalities / injuries
3	Maintaining current infrastructure
4	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment
2	Multi Modal Facilities



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No a least

5. Are there particular areas that need transportation improvements? Please be specific.

signal timing. often it feels like the system is set up to stop people, rather than keep people moving. Very noticeable in situations of low traffic volume

6. Are there particular areas that generate safety concerns? Please be specific.

- even though not identified as a top 10 crash location, 53 and Pleasantview is a nightmare. I avoid it.
- Prairie Avenue

7. What transportation issues concern you with respect to your community?

- would like the Greensferry Bridge: need to create an alternative south of river other than Spokane St.
- Timing of lights

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

access ~~near~~
more choices south of Spokane River
Greensferry Bridge, someday Pleasantview?
Bridge

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

along identified commuter routes connecting neighborhoods to communities/transit facilities.



POST FALLS HIGHWAY DISTRICT
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Public Involvement Questionnaire

Name: FRED GABOURIE	Email: idahoosprey@gmail.com
Address: PO Box 2529 Post Falls Id 83877	
Phone Number: 208 699-4944	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	X		Safety / Accidents
	X		Peak hour traffic congestion
	X		Maintenance
		X	Bicycle system
		X	Walkability / Pedestrian systems
	X		Truck traffic
	X		Traffic operations (i.e. traffic signal timing / coordination)
	X		Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Hwy 41 4 Lanes to Rathdrum
Signal @ LANCASTER / Hwy 41

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

Along Hwy 41, along Seltice way



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 208-765-3717

Public Involvement Questionnaire

Name: <u>Angela Comstock</u>	Email: <u>acomstock@jub.com</u>
Address: <u>15854 Middle Fork Rd, Hauser, ID 83854</u>	
Phone Number: <u>208 7559661</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Walkability / Pedestrian systems
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<u>2</u>	Maintenance / Repair of existing transportation infrastructure
<u>3</u>	New roads or added capacity on roads
<u>1</u>	Improved safety
	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<u>2</u>	Reducing fatalities / injuries
<u>1</u>	Maintaining current infrastructure
	Reducing congestion
<u>3</u>	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Pleasant view & 53 ; Prairie & P.view
Surface condition btw Chase & Idaho
on prairie

6. Are there particular areas that generate safety concerns? Please be specific.

Prairie & pleasant view
Hauser Lake Rd pedestrians/speed

7. What transportation issues concern you with respect to your community?

adequate budgeting for growing
construction cost

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

S. Greensferry

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

Prairie



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Public Involvement Questionnaire

Name: <u>Russ Connors</u>	Email: <u>rconnors@postfallsidaho.gov</u>
Address: <u>408 N. Spokane St.</u>	
Phone Number: <u>773-8708</u>	Check box if you would like a response to your comments <input checked="" type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input checked="" type="checkbox"/>	New roads or added capacity on roads
<input checked="" type="checkbox"/>	Improved safety
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input checked="" type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input checked="" type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
<input checked="" type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

- Spokane St. and Prairie
- Improve Beck Rd. with heavy truck traffic in mind

6. Are there particular areas that generate safety concerns? Please be specific.

- Pleasant View at Prairie and at Highway 53

7. What transportation issues concern you with respect to your community?

- Encourage Roundabouts at Arterial intersections
- Add multi-modal facilities on Arterials and collectors

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

- Properties south of Spokane River have limited access. Adding Greenway bridge would improve safety and reduce congestion on Spokane St.
- Also, bridge at Pleasant View may be worth considering in long term plan.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

- Prairie
- Collectors and up.



POST FALLS HIGHWAY DISTRICT
 5629 EAST SELTICE WAY
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 208-765-3717

Public Involvement Questionnaire

Name: <i>Edmar Hampton</i>	Email:
Address: <i>8990 W. Duvine</i>	
Phone Number: <i>208-273-339</i>	Check box if you would like a response to your comments <input type="checkbox"/>

Cal 208-691-5588

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good Fair Poor

Good	Fair	Poor	
	X		Safety / Accidents
	X		Peak hour traffic congestion
X			Maintenance
X			Bicycle system
		X	Walkability / Pedestrian systems
		X	Truck traffic
X			Traffic operations (i.e. traffic signal timing / coordination)
			Transit system

not enough traffic signals

2. Which 3 issues are most important to address in the Transportation Plan?

<i>2</i>	Maintenance / Repair of existing transportation infrastructure
<i>3</i>	New roads or added capacity on roads
	Improved safety
<i>1</i>	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<i>1</i>	Reducing fatalities / injuries
	Maintaining current infrastructure
	Reducing congestion
<i>2</i>	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



POST FALLS HIGHWAY DISTRICT
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208-765-3717

4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

Traffic lights on Greenfern, Chase,
Pleasant Views, & no roundabouts

6. Are there particular areas that generate safety concerns? Please be specific.

Prairie Ave. Use traffic signals

7. What transportation issues concern you with respect to your community?

Prepare for future growth - streets & bike paths

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

none - ~~just~~ just plan for future growth

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

near all schools



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Public Involvement Questionnaire

Name:	Tim R Turrell Sr	Email:	
Address:	2855 W. Yakon Ave		
Phone Number:	208-762-7304	Check box if you would like a response to your comments	<input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	✓		Safety / Accidents
	✓		Peak hour traffic congestion
✓			Maintenance
	✓		Bicycle system
	✓		Walkability / Pedestrian systems
	✓		Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
		✓	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

2	Maintenance / Repair of existing transportation infrastructure
3	New roads or added capacity on roads
1	Improved safety
	Improved road operations & traffic signal timing
	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

	Reducing fatalities / injuries
	Maintaining current infrastructure
X	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

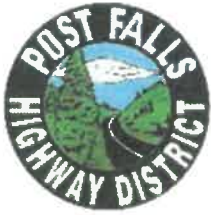
5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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Public Involvement Questionnaire

Name:	Chris Comstock	Email:	ccomstockstrata@gmail.com
Address:	15854 Middle Fork Rd Hauser 83854		
Phone Number:	208.770.9362	Check box if you would like a response to your comments	<input checked="" type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	X		Safety / Accidents
		X	Peak hour traffic congestion
	X		Maintenance
		X	Bicycle system
	X		Walkability / Pedestrian systems
		X	Truck traffic
	X		Traffic operations (i.e. traffic signal timing / coordination)
			Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or <u>added capacity on roads</u>
X	Improved safety - <u>prairie & pleasant view corridor</u>
X	Improved road operations & traffic signal timing ←
<input type="checkbox"/>	Improved road signage
<input type="checkbox"/>	Additional bicycle & pedestrian Facilities
X	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
X	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network ← and
<input type="checkbox"/>	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

Maybe. Doesn't "make sense" everywhere.

Develop a bike "Plan" to designate strategic widening

5. Are there particular areas that need transportation improvements? Please be specific.

Hwy 53 from state line past McGuire.

Including the Hauser junction area and these intersections

- H. Lake Road, Beck/Prairie, Pleasantview

6. Are there particular areas that generate safety concerns? Please be specific.

See above. What is ITO's position?

Also - Inaaster and Hwy 41

7. What transportation issues concern you with respect to your community?

- Turning onto ~~the~~ Hwy 53 from Hauser Lake Road.

- Providing "Connectivity" in the I-90 corridor. i.e. access over/under I-90 in key locations. For example

- Treaty Rock

- Seltice Crossing

- @ Hwy 41

- Spokane ST.

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

- Hwy 41 between Mullan and Seltice is congested.

- Railroad ~~crossing~~

Trying to cross back and forth over the tracks.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

- Better Connectivity around I-90 - local paths along Rail easements

- More "City" cycling & walking between parks. Don't need as many ~~facilities~~ facilities for 5+ mile cyclists, kids and strollers.

- Hard to Cross Seltice in the Chase - Idaho section



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Public Involvement Questionnaire

Name: <i>Joly Bray</i>	Email: <i>brayj@kqg.us</i>
Address: <i>400 NW Blvd, CPA, ID 83816</i>	
Phone Number: <i>208.446.1608</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor
	✓	
	✓	

- Safety / Accidents
- Peak hour traffic congestion
- Maintenance
- Bicycle system
- Walkability / Pedestrian systems
- Truck traffic
- Traffic operations (i.e. traffic signal timing / coordination)
- Transit system *☺ working to make it efficient, effective, safe*

Thank you for including us!

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- 3* Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- 2* Additional bicycle & pedestrian Facilities
- Truck routing & access

1 *Road design to accommodate buses, bicycles, pedestrian*

3. Which concept is most important for transportation planners to focus on?

- 1* Reducing fatalities / injuries
- Maintaining current infrastructure
- 2* Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment

3 *Public transportation*



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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Public Involvement Questionnaire

Name:	JANA & RICHARD FLORES	Email:	
Address:	620 N. COLES LOOP		83854
Phone Number:	714-851-4106	Check box if you would like a response to your comments	<input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
			Safety / Accidents
	✓		Peak hour traffic congestion
		✓	Maintenance
		✓	Bicycle system
		✓	Walkability / Pedestrian systems
		✓	Truck traffic
		✓	Traffic operations (i.e. traffic signal timing / coordination)
		✓	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

We need exit ramp to Huetter Rd.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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Public Involvement Questionnaire

Name:	James Steffensen	Email:	james.steffensen@yohua.com
Address:	8070 N Wentworth St PF 83854		
Phone Number:	910-262-8592	Check box if you would like a response to your comments	<input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Walkability / Pedestrian systems
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<input checked="" type="checkbox"/>	Maintenance / Repair of existing transportation infrastructure
<input checked="" type="checkbox"/>	New roads or added capacity on roads
<input type="checkbox"/>	Improved safety
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input checked="" type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input checked="" type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

The infrastructure for the continued growth of the city

6. Are there particular areas that generate safety concerns? Please be specific.

prairie Ave.

7. What transportation issues concern you with respect to your community?

safety and growth.

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

N/A

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

Seltice, broken sidewalks.



POST FALLS HIGHWAY DISTRICT
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Public Involvement Questionnaire

Name: <u>Marsha Marhofke</u>	Email: <u>gtxm1m@yahoo.com</u>
Address: <u>2337 N. Howell Rd Post Falls</u>	
Phone Number: <u>208 773-8877</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
✓			Safety / Accidents
		✓	Peak hour traffic congestion
✓			Maintenance
✓			Bicycle system
✓			Walkability / Pedestrian systems
	✓		Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
			Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

✓	Maintenance / Repair of existing transportation infrastructure
	New roads or added capacity on roads
	Improved safety
✓	Improved road operations & traffic signal timing
✓	Improved road signage
	Additional bicycle & pedestrian Facilities
	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

	Reducing fatalities / injuries
2	Maintaining current infrastructure
1	Reducing congestion
	Improving roadway reliability
	Improving the trucking network
	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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 208-765-3717

Public Involvement Questionnaire

Name: <u>MARHOFKE, JAMES</u>	Email: <u>JEMARHOFKE@HOTMAIL.COM</u>
Address: <u>2337 N. HOWELL RD.</u>	
Phone Number: <u>208-773-8877</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good Fair Poor

Good	Fair	Poor	
✓			Safety / Accidents
		✓	Peak hour traffic congestion
✓			Maintenance
	✓		Bicycle system
✓			Walkability / Pedestrian systems
	✓		Truck traffic
	✓		Traffic operations (i.e. traffic signal timing / coordination)
	✓		Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- 2 Maintaining current infrastructure
- 1 Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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Public Involvement Questionnaire

Name: <i>DEAN SUTTON</i>	Email: <i>deansutton72msn.com</i>
Address: <i>12459 W. MONUMENT DR, PF 83854</i>	
Phone Number: <i>208 777-2569</i>	Check box if you would like a response to your comments <input checked="" type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<i>1</i>	Maintenance / Repair of existing transportation infrastructure
<i>2</i>	New roads or added capacity on roads <i>(assuming Post Falls continues major growth)</i>
<i>3</i>	Improved safety <i>(such as roundabouts and flashers on steep signs @ strategic intersections)</i>
<input type="checkbox"/>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input type="checkbox"/>	Additional bicycle & pedestrian Facilities
<input type="checkbox"/>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input checked="" type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment

(only 1-3, how about rotary)



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?

Yes No

Only if bicyclists are required to use shoulders.

5. Are there particular areas that need transportation improvements? Please be specific.

Shilling loop - needs paved. Several areas along creek are deteriorating and dangerous. Continued grading necessary with increased traffic. This is a growth area.

6. Are there particular areas that generate safety concerns? Please be specific.

Corner of Shilling loop (w) and W Riverview - incoming traffic from Riverview will generally cut short and it would be good to have some form of channeling to keep traffic in lane.

7. What transportation issues concern you with respect to your community?

Continued grading + proper snow removal.

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

None

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

I wouldn't. This is creating a very dangerous situation.



POST FALLS HIGHWAY DISTRICT
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Public Involvement Questionnaire

Name: <i>Daniel Carmichael</i>	Email: <i>e8ty2nd@gmail.com</i>
Address: <i>7313 E. 2nd Ave Spokane Valley, WA 99212</i>	
Phone Number: <i>509.607.2804</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Safety / Accidents
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Peak hour traffic congestion
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Maintenance
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Bicycle system
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Walkability / Pedestrian systems
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Truck traffic
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Traffic operations (i.e. traffic signal timing / coordination)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

<i>1</i>	Maintenance / Repair of existing transportation infrastructure
<input type="checkbox"/>	New roads or added capacity on roads
<input type="checkbox"/>	Improved safety
<i>2</i>	Improved road operations & traffic signal timing
<input type="checkbox"/>	Improved road signage
<input type="checkbox"/>	Additional bicycle & pedestrian Facilities
<i>3</i>	Truck routing & access

3. Which concept is most important for transportation planners to focus on?

<input type="checkbox"/>	Reducing fatalities / injuries
<input type="checkbox"/>	Maintaining current infrastructure
<input type="checkbox"/>	Reducing congestion
<input checked="" type="checkbox"/>	Improving roadway reliability
<input type="checkbox"/>	Improving the trucking network
<input type="checkbox"/>	Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?

Lack of promotion to grow & use pedestrian/cyclist "trails" and/or shoulders connecting vital locations.

8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?



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 208-765-3717

Public Involvement Questionnaire

Name: ED ADAMCHAK JR	Email: adamchak@roadnumbr.com
Address: 719 S GREENSFERRY ROAD P.F	
Phone Number: 206-625-0428	Check box if you would like a response to your comments <input checked="" type="checkbox"/>

1. How would you rate the existing transportation system within the PFHD jurisdiction?

Good	Fair	Poor	
	X		Safety / Accidents
	X		Peak hour traffic congestion
		X	Maintenance
		X	Bicycle system
		X	Walkability / Pedestrian systems
	X		Truck traffic
	X		Traffic operations (i.e. traffic signal timing / coordination)
		X	Transit system

2. Which 3 issues are most important to address in the Transportation Plan?

- Maintenance / Repair of existing transportation infrastructure
- New roads or added capacity on roads
- Improved safety
- Improved road operations & traffic signal timing
- Improved road signage
- Additional bicycle & pedestrian Facilities
- Truck routing & access

3. Which concept is most important for transportation planners to focus on?

- Reducing fatalities / injuries
- Maintaining current infrastructure
- Reducing congestion
- Improving roadway reliability
- Improving the trucking network
- Protecting / enhancing the environment



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4. Should shoulders on the roadways be widened to provide a travel lane for bicyclists?
Yes No

5. Are there particular areas that need transportation improvements? Please be specific.

6. Are there particular areas that generate safety concerns? Please be specific.

7. What transportation issues concern you with respect to your community?


8. What portion(s) within the PFHD jurisdiction do you find difficult to access? Please be specific.

9. If you could put more cycling & pedestrian facilities anywhere within the PFHD jurisdiction where would you place them?

Appendix D
Technical Advisory Meeting #2

IMPORTANCE RANKING	1 SAFETY RANKING				2 REMAINING SERVICE LIFE				3 TIES TO ADJACENT PROJECT			4 AREA GROWTH			5 PROJECT AMOUNT			6 FUNDING SOURCE			7 PUBLIC SUPPORT		8 ROW NEEDS				9 UTILITY IMPACTS				FINAL SCORE MAX. 100	POTENTIAL FUNDING SOURCE (FOR PROJECTS WITH UNSECURED FUNDING)
	FATALITY / TYPE A	HIGH ACCIDENT	BIKE/ PED SAFETY	IMPROVE EMERG. ACCESS	EST. LIFE	11 TO 20	6 TO 10	0 TO 5	YES	NO	SOMEWHAT	HIGH	MOD.	LITTLE	< \$0.5 MIL	\$0.5 - \$2.0 MIL	> \$2.0 MIL	FUNDED	NOT FUNDED	SOURCE OF FUNDING / SCHEDULE	SPOKEN	UNSPOKEN	NONE	1 PARCEL	> 1 PARCEL	NONE	MOD (1-2)	HIGH 3+	UN KNOWN			
	20	10	5	5	5	10	15	15	15	0	10	15	10	0	10	5	2	10	0		5	0	5	3	0	5	3	0	0			
MAX. POINTS / CATEGORY	20				15				15			15			10			10			5		5				100					
PROJECTS	COST, MIL																															
BECK RD. KN 19288 Road Rehab, Widen Shoulders In Design	\$ 2.50					6														STP RURAL 2018 DESIGN 2022 CONST	5		5							0		
RIVERVIEW DRIVE Intersection Improvements at Idaho Rd"	\$ 0.50					10																0	?						0		0	
RIVERVIEW DRIVE Safety Improvements	\$ 0.45																													0	HB312/PFHD	
RIVERVIEW DRIVE Guard Rail Installation	\$ 0.45																			LHSIP DESIGN 2017 CONST 2018		0			0	5				5		
RIVERVIEW DRIVE Skalen Creek widening	\$ 1.00					NR														STP RURAL DESIGN 2017 CONST 2019		0		3			3			6		
RIVERVIEW DRIVE Curve Realignment at St. Dominics	\$ 0.50					NR															5			3		?				8		
RIVERVIEW DRIVE EXTENSION Foothills to Fairmont Loop	NR					NA																0			0				0	0		
MILLSAP LOOP Big Rock Road rebuild intersection and hill	\$ 0.05					10														PFHD 2021 DESIGN 2022 CONST	?		?			?				10		
HUETTER ROAD Mullan to Prairie widen shoulders & structural overlay, Install turn lanes at Mullan, Greta, Poleline & Big Sky	\$ 0.40					4														PFHD	?		?			?				4		
HAYDEN AVE & MEYER ROAD Intersection Improvements	\$ 1.20					10														STP RANKED 10TH	?				0				0	10		
PRAIRIE AVENUE 5 lanes from Meyer to SH-41	\$ 4.20					8															5		?				0		13			
PRAIRIE & CHASE RD Roundabout	\$ 0.50					NA														CONST 2018	5		5			5				15	STRATEGIC INITIATIVES	
PRAIRIE AVENUE SH-41 to Greensferry, CRABS & Overlay	\$ 0.30					10														PFHD	5		5			5				25		
PRAIRIE & GREENSFERRY Intersection Improvements	\$ 1.20					10															5		?			?				15		
PLEASANT VIEW & PRAIRIE Intersection Improvements	\$ 1.20					8														LHSIP DESIGN 2018 CONST 2019	5		?			?				13		
PLEASANT VIEW & SH-53 Intersection Improvements	\$ 2.00					NR															5		?			?			5			
SELTICE WAY Seeley St to Huetter Overlay	\$ 0.45					6															5		5			?				16		

Appendix E
Technical Advisory Meeting #3

 POST FALLS HIGHWAY DISTRICT CAPITAL IMPROVEMENTS PLAN DRAFT 2018-2022				IMPORTANCE RANKING RATING CATEGORIES MAX. POINTS / CATEGORY			1 SAFETY RANKING FATALITY / TYPE A HIGH ACCIDENT BIKE / PED SAFETY IMPROVE EMERG. ACCESS					2 TRANSPORTATION/CAPACITY CAPACITY TRUCK ROUTES BIKE/PED			3 ECONOMIC VITALITY Significant Impact Some Impact No Impact			4 REMAINING SERVICE LIFE 11 TO 20 6 TO 10 0 TO 5			5 PUBLIC SUPPORT WIDE-SPREAD SUPPORT SOME PUBLIC SUPPORT UNSPOKEN			6 TIES TO ADJACENT PROJECT YES NO SOMEWHAT			7 PROJECT AMOUNT < \$0.5 MIL \$0.5 - \$2.0 MIL > \$2.0 MIL			8 ROW NEEDS NONE 1 PARCEL > 1 PARCEL			FINAL SCORE MAX. 100
				25					15			15			15			10			10			5			5			100			
FUNDING STATUS	FUNDING SOURCE	YR SCHEDULED	PROJECTS	COST, MIL																													
APPROVED FUNDING	STP RURAL	2017 - 2022	BECK RD. KN 19288 Road Rehab, Widen Shoulders In Design	\$ 2.50																													
APPROVED FUNDING	PFHD	2017 - 2019	WYOMING AVE Meyer Rd to Huetter rebuild to gravel standards	\$ 0.10																													
APPROVED FUNDING	PFHD	2017 - 2018	HUETTER ROAD Mullan to Prairie widen shoulders & structural overlay, install turn lanes at Mullan, Greta, Poleline & Big Sky	\$ 0.40																													
APPROVED FUNDING	PFHD	2017 - 2018	HAUSER LAKE ROAD Culvert Sizing / Elevate Roadway	\$ 0.30																													
APPROVED FUNDING	PFHD	2021 - 2022	MILLSAP LOOP Big Rock Rd rebuild to intersection & hill	\$ 0.50																													
	STP/HB312, PFHD	PRELIMINARY	PRAIRIE AVENUE 5 lanes from Meyer to SH-41	\$ 4.20	0.0	4.7	7.2	4.2		3.2	6.6	3.6	5.6	4.9	0.0	0.0	8.9	1.1	7.8	2.1	0.0	7.8	0.0	2.2	0.2	2.2	0.2	1.1	0.7	0.0	74.2		
	PFHD	2019	PRAIRIE AVENUE SH-41 to Greensferry, CRABS & Overlay	\$ 0.30	0.0	3.2	3.2	0.8		0.9	5.0	0.4	0.9	4.4	1.7	0.0	10.0	0.0	5.6	3.3	0.0	6.7	1.7	1.1	0.6	2.6	0.6	3.9	0.6	0.0	57.0		
	HB312,PFHD	2020	SELTICE WAY Seeley St to Huetter Overlay	\$ 0.45	0.0	2.7	1.4	0.4		0.2	2.8	0.6	0.0	3.3	0.8	0.0	10.6	0.0	0.0	5.4	0.0	5.6	1.1	1.1	0.6	3.1	0.0	4.4	0.6	0.0	44.7		
	HB312,PFHD	2021	RIVERVIEW DRIVE Curve Realignment at St. Dominics	\$ 0.50	0.0	2.6	4.7	1.2		0.7	1.2	1.9	0.0	1.7	0.4	0.6	2.8	0.0	0.0	6.7	0.0	0.9	0.0	3.6	0.9	2.3	0.0	0.0	3.2	0.0	35.2		
		PRELIMINARY	RIVERVIEW DRIVE EXTENSION Foothills to Fairmont Loop	NR	0.0	2.1	2.6	2.7		1.9	1.4	3.7	0.0	2.1	1.0	0.6	1.1	0.0	0.0	3.0	0.2	6.4	0.0	1.0	0.0	2.6	0.2	0.0	0.0	0.0	32.6		
		PRELIMINARY	UPRIVER DRIVE Realignment at Jacobs Loop	\$ 0.15	0.0	4.6	4.1	1.0		1.1	1.3	1.2	0.0	1.1	0.4	0.0	1.7	0.6	0.0	2.3	0.4	1.7	0.2	1.9	2.2	1.1	0.0	0.6	0.7	0.2	28.4		
APPROVED FUNDING	LHSIP	2018 - 2020	PLEASANT VIEW & PRAIRIE Intersection Improvements	\$ 1.20																													
APPROVED FUNDING	STRATEGIC INITIATIVES	2018	PRAIRIE & CHASE RD Roundabout	\$ 1.00																													
	LHSIP/STP/PFHD	2021	PRAIRIE & GREENSFERRY Intersection Improvements	\$ 1.20	2.8	6.8	8.0	2.6		2.3	5.2	1.0	2.8	2.8	0.3	0.0	9.4	0.0	0.0	5.8	0.6	6.7	1.4	1.1	0.0	3.2	0.0	0.6	0.6	0.0	63.9		
	TIGER	-	PLEASANT VIEW & SH-53 Intersection Improvements	\$ 2.00	1.3	8.3	4.6	2.5		2.3	9.4	1.1	3.8	7.4	0.0	0.0	3.1	0.0	1.9	6.9	0.0	1.3	3.8	2.5	0.0	2.8	0.5	0.0	0.6	0.0	63.8		
	STP, PFHD	2021	HAYDEN AVE & MEYER ROAD Intersection Improvements	\$ 1.20	0.0	8.9	4.2	2.0		3.0	4.1	1.3	1.7	3.7	0.6	0.0	9.4	0.0	2.0	2.7	0.0	0.0	1.7	3.8	0.0	3.2	0.0	0.0	0.2	0.0	52.4		
		PRELIMINARY	RIVERVIEW DRIVE Intersection Improvements at Idaho Rd	\$ 0.50	0.0	6.5	2.3	0.8		2.8	0.4	1.1	0.0	1.3	0.4	0.0	8.1	0.0	0.0	3.0	0.0	0.9	0.6	2.3	1.9	1.3	0.0	0.6	0.6	0.8	35.4		
		PRELIMINARY	RIVERVIEW DRIVE-HARBOR DRIVE Intersection Improvements	\$ 0.15	0.0	5.1	1.7	0.7		0.6	1.0	1.0	0.0	1.6	0.3	0.0	1.1	0.0	0.0	0.6	0.8	0.0	0.6	1.3	1.8	1.1	0.0	1.1	1.7	0.0	21.9		

ROAD IMPROVEMENTS

INTERSECTION IMPROVEMENTS

Appendix F
Open House #2



Post Falls Highway District Transportation Plan Open House

Post Falls Highway District
is seeking public input
on their
Draft Capital Improvement Plan.

Please join us

Tuesday, January 9, 2018

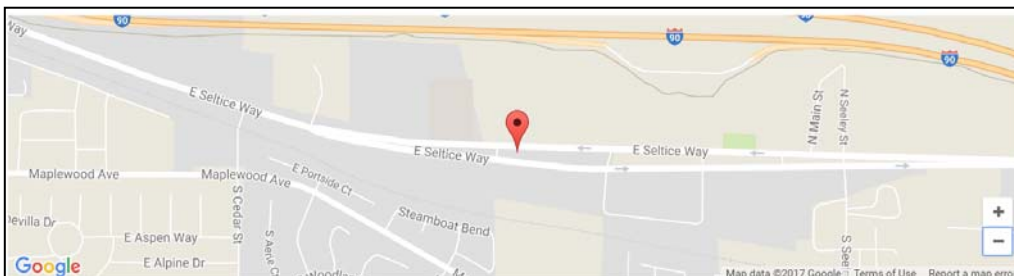
4:00 pm to 7:00 pm

Post Falls Highway District Office

5629 E. Seltice Way

Post Falls, Idaho 83854

208-765-3717



POST FALLS CONSIDERS BRINGING BRIDGE BACK

January 06, 2018 at 5:00 am |



LOREN BENOIT/Press A high-profile project the Post Falls Highway District is embarking on is a single-lane roundabout at the intersection of Pleasantview Road and Prairie Avenue, a spot notorious for serious accidents and fatalities in recent years. At one point, there were 14 accidents at the intersection in 10 months.

By BRIAN WALKER

**POST FALLS HIGHWAY OPEN
HOUSE**

Staff Writer

POST FALLS — Fifty years after the Greensferry bridge over the Spokane River at Post Falls closed, building another structure at the same location is being explored.

"(The Post Falls Highway District) is in the very preliminary stages of looking at the possibility of building a bridge at Greensferry," said Kelly Brownsberger, the district's road supervisor. "The district engineer is evaluating the site to make sure a bridge will fit in the existing right of way."

The Post Falls Highway District will host a public input open house for its draft project plan on Tuesday from 4 to 7 p.m. to accept comments on future roadways and improvements in the district. The open house is at the district office at 5629 E. Seltice Way.

The bridge is among 26 projects listed on the district's draft project list that will be open for public comments during an open house on Tuesday from 4 to 7 p.m. at the district office at 5629 E. Seltice Way.

The cost for the bridge is estimated at \$8.5 million, but a construction date has not been determined as the district is only exploring the possibility at this point.

"One of the biggest hurdles we're facing is funding," Brownsberger said. "Currently there is no federal or state grants available to build a new bridge. We are looking into the possibility of passing a bond for construction, but everything is very preliminary right now."

The old bridge was closed in September 1967 because it was in poor condition. It was dismantled in 1971.

Proponents of building another bridge there say it would improve mobility in that area as the only way to access south side of the river from Post Falls is the Spokane Street bridge. The Greensferry bridge would also improve emergency response and delivery of goods and services.

If the bridge were to become reality, motorists could travel from the south side of the river to the Rathdrum Prairie in a straight shot with the Greensferry overpass that was constructed over Interstate 90 two years ago.

A high-profile project the district is embarking on is a single-lane roundabout at the intersection of Pleasantview Road and Prairie Avenue, a spot notorious for serious accidents and fatalities in recent years.

At one point, there were 14 accidents at the intersection in 10 months.

"Design of the roundabout is just getting started with construction scheduled in 2020," Brownsberger said, adding that the cost is estimated at \$1.2 million.

Right of way will be purchased as part of the project, Brownsberger said.

"The district did an evaluation of possible improvements at the intersection, including a traffic signal," Brownsberger said. "We went with a roundabout because of the proven reduction in crashes that a roundabout provides. A traffic signal does not prevent the high-speed angle crashes that have been happening in the intersection.

"Also, with all the overhead clutter from all the power lines along with rail crossings in the area, it would be very easy to miss a traffic signal."

The roundabout will be designed to accommodate large trucks, Brownsberger said.

"There is currently a lot of truck traffic on Pleasantview from Highway 53 to Interstate 90 and the district commissioners have insisted that the roundabout be designed to handle large trucks," he said.

The highway district earlier made a series of improvements at the intersection to reduce accidents, including adding rumble strips, flashing lights, additional signage, larger-than-normal stop signs and surface leveling.

Another roundabout on Prairie Avenue — at Chase Road — was slated for construction last year but was bumped when the bids were twice the engineer's estimate. Brownsberger said the district has applied for state funding for the \$1 million project and, if approved, it is expected to be built starting late spring.

"This will have a major impact on traffic because the intersection will be closed during construction," he said. "We are going to keep the closure as short as possible, but it could be closed for up to 60 days."

Another project planned for this year is a \$400,000 widening and overlay of Huetter Road from Prairie to Mullan Avenue.

"This will cause some short-term traffic delays," Brownsberger said.

At the open house, residents will be able to review all of the projects and comment on how they're rated by the district.



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects Comment Sheet (Summary of Responses)

1. Do you have any comments concerning the projects listed on the CIP?

- There was a total of 9 comments that were directly in favor of the Greensferry Bridge, 2 that were opposed, and one that didn't care as long as his taxes did not go up.
- 1 Comment in favor of the Pleasant View / Prairie Roundabout
- 2 comments on bike/pedestrian access
- 1 comment on congestion on SH-41
- Feedback, for the most part, was positive and in favor of the projects listed on the CIP.

2. Are there any projects NOT listed on the CIP that you would like to be considered?

- Pleasantview Bridge over Spokane River
- Pleasantview Road multi-use pathways/HWY 53 multi-use pathways
- Accelerate the vertical curve problem at Riverview & Idaho
- Roundabouts at Pleasantview & Prairie and Poleline & Cecil
- At grade ramps at Greensferry and I-90

3. Do you have any comments concerning the rating of the projects on the CIP?

- Suggest increasing the rating W. Riverview & Idaho intersection below the proposed roundabouts
- How does PFHD prioritize funding for different improvement categories
- Greensferry Bridge should be #1
- The ratings appear to be correct / looks great

4. Do you have any comments concerning the funding of the projects on the CIP?

- Private funds for Greensferry Bridge available if at grade on/off ramps at Greensferry are made possible.
- Why should Post Falls be the only one to fund, many would use the Greensferry Bridge
- Grants, other?
- Bridge funding should be top priority



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects
Comment Sheet (Summary of Responses)

5. Do you have any other comments?

- Coordinate Hwy 53 / Huetter Bypass / Pleasant View / Beck Rd
- Explore options to use Beck Rd / Pleasant View & route to Lancaster to relieve Hwy 95 & Hwy 41 traffic
- Explore alternatives to relieve congestion on SH-41.
- Prefer stop lights/signs to roundabout (more cost effective).
- Concerns with roundabout at Prairie and Pleasant View for large trucks
- Need additional I-90 access between SH 41 and US 95
- Concerns with future traffic flow due to continued development and railroad traffic.
- Don't work on roads twice
- Post Falls does an excellent job – thank you!
- Thank you
- Good to see bike/ped projects
- Seem to be on top of your district & do a good job
- Fix the signals to keep traffic flowing smoothly
- Concerns about fire access south of the river if Spokane Street bridge were unusable.
- Would like to see Greensferry Bridge as soon as possible



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <u>John H. Kinney</u>	Email: <u>JHKIND@HOTMAIL.COM</u>
Address: <u>312 S. RICHARDS RD., POST FALLS, ID</u>	
Phone Number: <u>208 773-1894</u>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Highway 12 FAVOR OF PERSPECTIVE VIEW / PROPOSED INTERSECTION - INSTALL A ROUND-A-CORNER

2. Are there any projects NOT listed on the CIP that you would like to be considered?

INTERSECTION OF HARDISON'S WEST RIVERVIEW / SERIOUS VERTICAL CURVE PROBLEM ON N. RIVERVIEW

3. Do you have any comments concerning the rating of the projects on the CIP?

SUGGEST PLACING (RATING) THE WEST RIVERVIEW / IDAHO INTERSECTION JUST BELOW THE PROPOSED ROUNDABOUT PROJECTS.

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: Bryan D. Myers	Email: Bryand.Myers@gmail.com
Address: 11610 N SUNRISE CT, RATHDRUM ID	
Phone Number: 208 446 9988	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Integrate bike ped facilities where possible & secure ROW for future multi use paths & sidewalks as our community grows and becomes more URBAN. Focus on intersections and integrating w/ adjacent municipal road sections

2. Are there any projects NOT listed on the CIP that you would like to be considered?

Pleasant view Bridge @ Spokane River

Pleasant view Road multi use PATHWAYS

Hwy 53 Multi use Path

3. Do you have any comments concerning the rating of the projects on the CIP?

Emphasis on PED/BIKE FACILITIES OVERALL

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Coordination of Hwy 53 / HATTER BYPASS / PLEASANT VIEW / BECK ROAD
EXPLORE OPTIONS TO USE BECK ROAD / PLEASANT VIEW & ROUTE TO LANCASTER TO RELIEVE HWY 95 HWY 41 TRAFFIC

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	<i>Camy Nyssonom</i>	Email:	<i>Rndm250@mol.com</i>
Address:	<i>7024 W. Big Sky Dr</i>		
Phone Number:	<i>208 765 6384</i>	Check box if you would like a response to your comments	<input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?
2. Are there any projects NOT listed on the CIP that you would like to be considered?
3. Do you have any comments concerning the rating of the projects on the CIP?
4. Do you have any comments concerning the funding of the projects on the CIP?
5. Do you have any other comments?

Don't work on Roads Twice

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Dave Fair</i>	Email: <i>dfair@postfallsidaho.org</i>
Address: <i>3875 N. Chase Rd P.F. Id 83854</i>	
Phone Number: <i>208-773-7445</i> Check box if you would like a response to your comments <input type="checkbox"/>	

1. Do you have any comments concerning the projects listed on the CIP?

*The bike / Ped Issues must be addressed
sidewalks to connect to city infrastructure - ADA*

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Ron Hotchkiss</i>	Email: <i>KISS ES KORNER@RoadRunner.com</i>
Address: <i>8146 W Riverview DR</i>	
Phone Number: <i>208 667-0916</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Build the bridge

2. Are there any projects NOT listed on the CIP that you would like to be considered?

NO

3. Do you have any comments concerning the rating of the projects on the CIP?

NO

4. Do you have any comments concerning the funding of the projects on the CIP?

NO

5. Do you have any other comments?

Post Falls Highway Dept does an excellent job. Thank you!

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: Ron Hotchkiss Sally Hotchkiss	Email:
Address: 8146 W Riverview Dr Edw ID 83814	
Phone Number: (208) 607-0914	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Build The bridge

2. Are there any projects NOT listed on the CIP that you would like to be considered?

NO

3. Do you have any comments concerning the rating of the projects on the CIP?

NO

4. Do you have any comments concerning the funding of the projects on the CIP?

NO.

5. Do you have any other comments?

Thank you

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Angela Comstock</i>	Email: <i>acomstock@jub.com</i>
Address: <i>15854 N Middle Fork Rd, Hauser Id</i>	
Phone Number: <i>208-777-1532</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Projects seem concentrated in 2018-2020; not much beyond 2020

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

Looks like a good spread of options

5. Do you have any other comments?

Good to see bike/ped projects!

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Jay Hassell</i>	Email: <i>ahassell@job.com</i>
Address: <i>5785 N Harcourt Dr. COA (eastern most home in PFHD)</i>	
Phone Number: <i>659-9058</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Of all the projects I would support the rebuilding of the Greenberry Bridge across the Spokane River

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

How does the PFHD prioritize funding of different Improvement categories?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	Email:
Bob Flowers	gearup1956@gmail.com
Address:	
3914 E. Maplewood Ave. Post Falls Id 83854	
Phone Number:	Check box if you would like a response to your comments <input type="checkbox"/>
208-691-6420	

1. Do you have any comments concerning the projects listed on the CIP?

Prairie - Chase Roundabout

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	Email:
MIKE GARRON	
Address:	
PO BOX 2065 - CDA ID. 83816	
Phone Number:	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?
2. Are there any projects NOT listed on the CIP that you would like to be considered?
no
3. Do you have any comments concerning the rating of the projects on the CIP?
4. Do you have any comments concerning the funding of the projects on the CIP?
5. Do you have any other comments?

you seem to be on top of your dist. + do a good job

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>STEPHEN WASSON</i>	Email: <i>Stephenjwasson@gmail.com</i>
Address: <i>8642 W. DRAFTWOOD DRIVE</i>	
Phone Number: <i>(509) 216-6710</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

It's exciting to see what is being considered.

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

EXCITED FOR THE POSSIBILITY OF A BRIDGE @ GREENSFERRY, SEEMS TO MAKE SENSE.

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	Jeff Wilhelm	Email:	JJ12wil@yahoo.com
Address:	6669 W Harbor Dr. CDA, ID 83814		
Phone Number:	208-964-0236	Check box if you would like a response to your comments	<input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

I think we need the greenstevy bridge
This will greatly improve response time for fire & medical
This also will reduce traffic on Spokane Street

2. Are there any projects NOT listed on the CIP that you would like to be considered?

No

3. Do you have any comments concerning the rating of the projects on the CIP?

I would like the bridge to at least try and
get the funding as soon as possible.

4. Do you have any comments concerning the funding of the projects on the CIP?

No

5. Do you have any other comments?

No

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>LARRY ESTERLY</i>	Email: <i>Frogbottom@gmail.com</i>
Address: <i>309 E 13th Post Falls, Idaho 83854</i>	
Phone Number: <i>208 819 5177</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

I don't care what improvements you do as long as you "DON'T" raise my taxes (fixed income here)

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Hire someone with some smarts (other than state) to fix the stupid signal lights in Kootenai County and state inter sections

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854

Over



Post Falls Highway District
Open House
January 9th, 2018
Capital Improvement Plan Projects
Comment Sheet

*Respond by
mail
Please*

Name:	<i>Carla Skinner</i>	Email:	
Address:	<i>2127 Clarkfork Pkwy P.F. ID</i>		
Phone Number:		Check box if you would like a response to your comments	<input checked="" type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Greensferry being made a straight shot to Rathdrum. Please deal with congestion on 41. Why put apartments there.

2. Are there any projects NOT listed on the CIP that you would like to be considered?

An on and off ramps for 190 to help take pressure of 41.

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

Why should P.F. be the only one to fund. Those who live across should help pay for bridge

5. Do you have any other comments?

*(Safety issue)
Roundabout on Prairie & Pleasantview, 18 wheelers can not stop quickly when a car tries to race them into a roundabout. Consider 18 wheelers - liquified loads. Wouldn't a stop light be ~~more~~ better*

Please return Public Comments by January 16, 2018.

**Post Falls Highway District
 5629 E. Seltice Way
 Post Falls, Idaho 83854**

Why can't we get a response from Police to traffic on Clarkfork. School zone 20 mph cars doing 40 or more in high gear.



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Georgia Franklin</i>	Email:
Address: <i>1620 W FORDHAM ST, PF</i>	
Phone Number: <i>208 457-8001</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Can housing south of river contribute to bridge cost - as most are COA?

2. Are there any projects NOT listed on the CIP that you would like to be considered?

sound abouts @ Pleasantville Prairie

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

Need I-90 access (additional) between #41 & #95

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	LYNN BORDERS	Email:	LRBORDERS@GMAIL.COM
Address:	PO BOX 545, POST FALLS, ID 83877		
Phone Number:	208-773-6981	Check box if you would like a response to your comments	<input type="checkbox"/>

- 1. Do you have any comments concerning the projects listed on the CIP?**
LIKE THE PROJECTS. FUNDING LOOKS LIKE THE BIGGEST ISSUE.
- 2. Are there any projects NOT listed on the CIP that you would like to be considered?**
ROUND-A-BOUT AT POLELINE + CECIL
~~AB~~
- 3. Do you have any comments concerning the rating of the projects on the CIP?**
LOOKS GREAT.
- 4. Do you have any comments concerning the funding of the projects on the CIP?**
GRANTS, OTHER?
- 5. Do you have any other comments?**
NONE

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	MIKE MCFARLAND	Email:	MSMCFARLAND@KCGOV.ED
Address:	KCSO 5500 N GOV'T. WAY / PO Box 9000 COEUR D'ALENE, ID. 83816-9000		
Phone Number:	208) 446-1300	Check box if you would like a response to your comments	<input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

IT APPEARS THE PROJECTS HAVE BEEN PLACED IN PROPER PRIORITY

2. Are there any projects NOT listed on the CIP that you would like to be considered?

BASED ON WHAT I COULD SEE, ALL THE HOBI RECURRING PROBLEM AREAS ARE BEING ADDRESSED.

3. Do you have any comments concerning the rating of the projects on the CIP?

IT APPEARS BASED ON HISTORICAL ISSUES THE RATINGS ARE CORRECT.

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

THE ONLY THING WOULD BE THINKING OF FUTURE TRAFFIC FLOW ISSUES BASED ON CONTINUED DEVELOPMENT AND RAILROAD TRAFFIC.

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	ELMER KINGSTON	Email:	
Address:	8990 W. Prairie		
Phone Number:	208-691-5588	Check box if you would like a response to your comments	<input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

The bridge on Greenberry needs to be constructed as soon as possible.

2. Are there any projects NOT listed on the CIP that you would like to be considered?

No

3. Do you have any comments concerning the rating of the projects on the CIP?

The bridge should be #1.

4. Do you have any comments concerning the funding of the projects on the CIP?

Funding for the bridges should be a top priority

5. Do you have any other comments?

If for some reason the Spc St. bridge was unusable & a fire occurred on S. side the Lia. would be enormous

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Dave Runey</i>	Email: <i>DifferentialDave33@gmail.com</i>
Address: <i>8662 Highland</i>	
Phone Number: <i>208 773 4094</i>	Check box if you would like a response to your comments <input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?
2. Are there any projects NOT listed on the CIP that you would like to be considered?
3. Do you have any comments concerning the rating of the projects on the CIP?
4. Do you have any comments concerning the funding of the projects on the CIP?
5. Do you have any other comments? *I would like to see the green ferry Bridge put back, as soon as possible*

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name:	SCOTT WESTON	Email:	POWESTON57@GMAIL.COM
Address:	N. 16660 RIGHT FORK RD. HAUSER		
Phone Number:	208-773-9420	Check box if you would like a response to your comments	<input type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

NO

2. Are there any projects NOT listed on the CIP that you would like to be considered?

NOT SURE

3. Do you have any comments concerning the rating of the projects on the CIP?

NOT YET

4. Do you have any comments concerning the funding of the projects on the CIP?

NO

5. Do you have any other comments?

NO

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <i>Kineta Bethke</i>	Email: <i>bethkesroost@hotmail.com</i>
Address: <i>7412 Patrick Dr. CdA</i>	
Phone Number: <i>208-818-1852</i>	Check box if you would like a response to your comments <input checked="" type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

Prefer that you do not build bridge. huge cost. few folks would benefit. Already 2 bridges

2. Are there any projects NOT listed on the CIP that you would like to be considered?

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

5. Do you have any other comments?

*Roundabouts - please put in stop signs or lights
so much more cost effective!*

Please return Public Comments by January 16, 2018.

Post Falls Highway District

5629 E. Seltice Way

Post Falls, Idaho 83854

please put in ~~but~~ Bike paths where-ever possible!

From: Kelly Brownsberger [<mailto:kelly@postfallshd.com>]
Sent: Wednesday, January 10, 2018 7:45 AM
To: Laura Winter <lwinter@ruenyeager.com>
Subject: FW: New Form Entry: Contact Form

Good morning,

Would you add this to the comments received.

Thanks,

Kelly

From: Mary Spray [<mailto:mary@postfallshd.com>]
Sent: Wednesday, January 10, 2018 5:58 AM
To: 'Kelly Brownsberger' <kelly@PostFallsHD.com>; 'Shirley Walson' <shirley@postfallshd.com>
Subject: FW: New Form Entry: Contact Form

From: vicki@positivelypr.com [<mailto:no-reply@editmysite.com>]
Sent: Tuesday, January 09, 2018 3:13 PM
To: info@postfallshd.com
Subject: New Form Entry: Contact Form

You've just received a new submission to your [Contact Form](#).

Submitted Information:

Name

Jim and Vicki Larson

Email

vicki@positivelypr.com

Comment

We are unable to attend the open house tonight, Jan.9, 2018, but we completely and vigorously oppose the rebuilding of the Greens Ferry bridge. We live on W. George Lane and the bridge would come right by our house. We are 5 minutes from the Spokane Street bridge, and to spend \$8.5 million+ to save 5 minutes is absolutely ludicrous. It would also completely change the character and safety of our neighborhood, and make our property undesirable. Please take this into consideration, as we can hardly even believe this is being contemplated! Sincerely, Jim and Vicki Larson

Post Falls Highway District (PFHD) - Capital Improvement Plan Projects Comments

Submitted by:

Name: Joe & Corina Brown

Address: 7710 E Marine Dr., Post Falls, ID 83854

Email: joebrown@gmail.com

Phone: (208) 996-0188

1. Do you have any comments concerning the projects listed on the CIP?

The GREENSFERRY BRIDGE project is unnecessary and will have significant negative safety impacts to the community along Greensferry Road. Further, it is disappointing to see so few projects focused on safety improvements, economic improvements and biking/pedestrian improvements. We would have hoped the focus of the PFHD would be to improve the safety and vitality of our community, but that clearly does not seem to be the case with an organization willing to spend millions on an unnecessary bridge project.

2. Are there any projects NOT listed on the CIP that you would like to be considered?

Nearly every intersection on Hwy 41 and Hwy 53 can use significant improvement. These highways have become so congested, it has pushed traffic onto secondary roads that cannot support the load such as Huetter, Meyer, Greensferry and Idaho. Where these roads intersect with Hayden and Prairie respectively, significant safety improvements must be made. The area is growing quickly, and if PFHD does not make significant safety improvements at these key intersections now, safety and economic development in our community will suffer. Please, focus your capital budget in those areas.

3. Do you have any comments concerning the rating of the projects on the CIP?

The ratings are arbitrary. At the open house we asked how the matrix was developed. It was not developed following state or federal guidelines, good engineering practice, or stakeholder engagement standards. So, we ask, how was it developed? As stated previously, The GREENSFERRY BRIDGE Project is a waste of our tax payer money, so we fail to see how the economic vitality rating is so high. The negative safety issues introduced by increasing traffic on Greensferry Road south of Seltice Way far outweighs any safety improvements afforded by increased access south of the river. Focus on the Spokane Street Bridge and making needed improvements to existing infrastructure.

4. Do you have any comments concerning the funding of the projects on the CIP?

The GREENSFERRY BRIDGE is not needed and a waste of our money. Whether this is funding that comes from local taxes, federal taxes or fees, funding the GREENSFERRY BRIDGE Project is a waste. Please do not waste any of our money on something so frivolous that negatively impacts so many and benefits very few.

5. Do you have any other comments?

Instead of adding more infrastructure (e.g. GREENSFERRY BRIDGE Project Proposal), focus on and fix what we already have. Focus on improving the safety and economic vitality of our communities. We suggest rather than getting very narrow feedback from only a few individuals involved in roads projects, as was described by the engineer at the Open House, that the PFHD involve the community in comprehensive planning. Developing a long term plan for our community with projects we need would be beneficial. We ask that you keep us apprised of the GREENSFERRY BRIDGE Project, and ask that PFHD stop wasting our money on a bad project.



Post Falls Highway District

Open House

January 9th, 2018

Capital Improvement Plan Projects

Comment Sheet

Name: <u>JIM ROLETTO</u>	Email: <u>jim@enricoconsulting.com</u>
Address: <u>8965 N FINOCANE DR</u>	
Phone Number: <u>208 967 4232</u>	Check box if you would like a response to your comments <input checked="" type="checkbox"/>

1. Do you have any comments concerning the projects listed on the CIP?

~~Yes~~ No

2. Are there any projects NOT listed on the CIP that you would like to be considered?

YES, at grade on ramps both East & West bound I-90 @ GREEN FERRY

3. Do you have any comments concerning the rating of the projects on the CIP?

4. Do you have any comments concerning the funding of the projects on the CIP?

Greenferry
The bridge project over the Spokane river funding could be available through private

5. Do you have any other comments?

* a private company is very willing to discuss a loan for constructing the bridge on Greenferry over the Spokane river if at grade on ramps from I-90 on Greenferry road is possible

Please return Public Comments by January 16, 2018.

Post Falls Highway District
5629 E. Seltice Way
Post Falls, Idaho 83854

From: Kelly Brownsberger [<mailto:kelly@postfallshd.com>]
Sent: Wednesday, January 10, 2018 7:45 AM
To: Laura Winter <lwinter@ruenyeager.com>
Subject: FW: New Form Entry: Contact Form

Good morning,

Would you add this to the comments received.

Thanks,

Kelly

From: Mary Spray [<mailto:mary@postfallshd.com>]
Sent: Wednesday, January 10, 2018 5:58 AM
To: 'Kelly Brownsberger' <kelly@PostFallsHD.com>; 'Shirley Walson' <shirley@postfallshd.com>
Subject: FW: New Form Entry: Contact Form

From: vicki@positivelypr.com [<mailto:no-reply@editmysite.com>]
Sent: Tuesday, January 09, 2018 3:13 PM
To: info@postfallshd.com
Subject: New Form Entry: Contact Form

You've just received a new submission to your [Contact Form](#).

Submitted Information:

Name

Jim and Vicki Larson

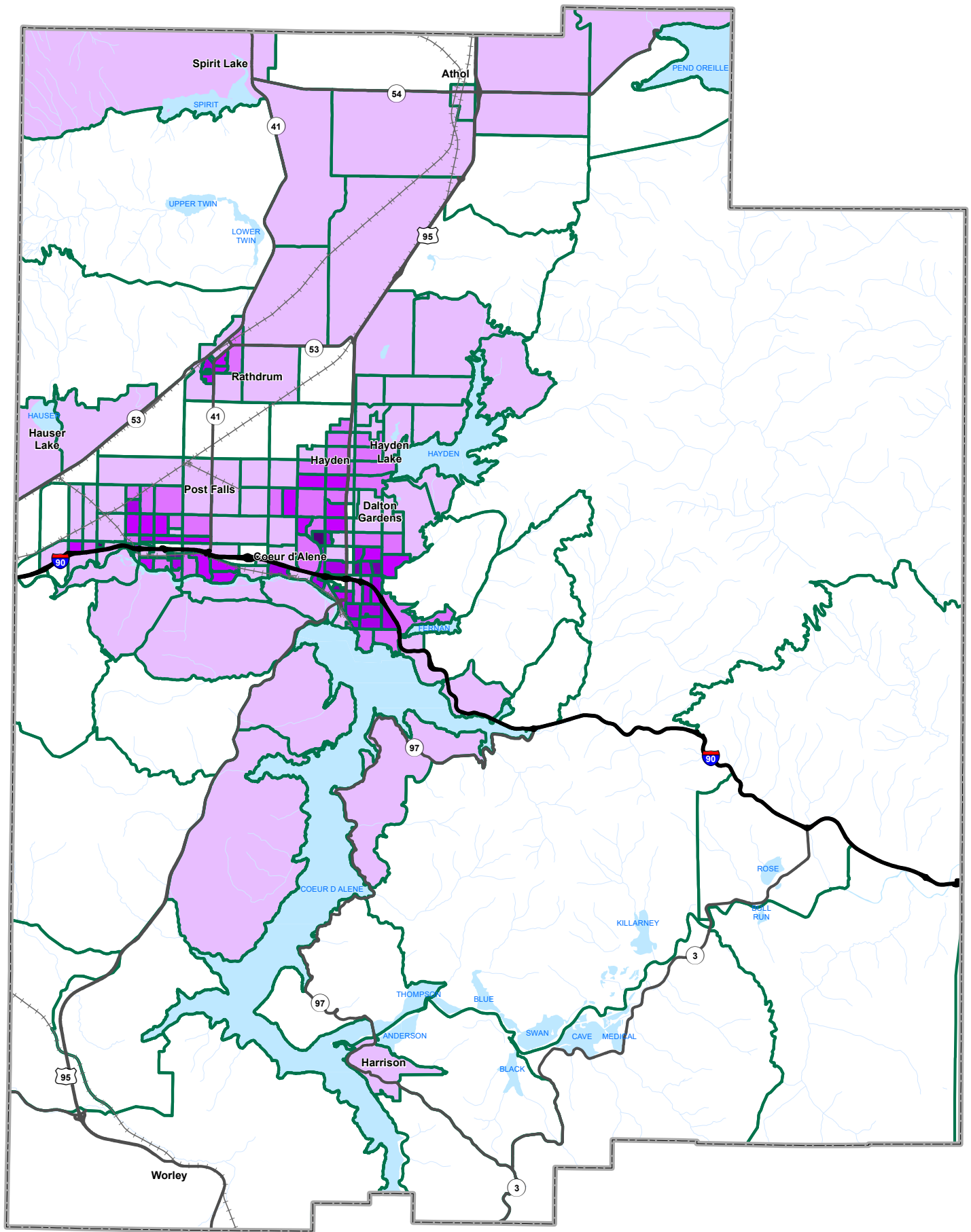
Email

vicki@positivelypr.com

Comment

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Appendix G
Population Data

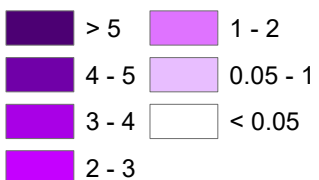


*Data based on best available information

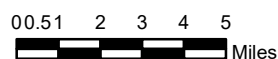
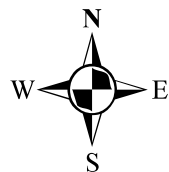
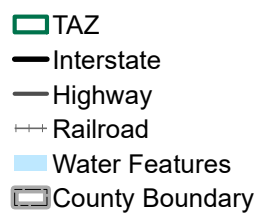
*Data for illustrative purposes only

Population Density by Traffic Analysis Zone (TAZ) - 2016

Households Per Acre



Other Features



Jurisdiction	2010 Historic Annual Growth Rates								PROPOSED (3) Average Annual Growth Rates Proposed for Model Projections	COMPARISON ONLY		2014 (4) Total Population	Total Increase 2010-2014			2020			Total Increase 2010-2020			PROPOSED (3) Average Annual Growth Rates Proposed for Model Projections	2035				Total Increase 2010-2035				
	1990 Census Population	2000 Census Population	(1) 2010 Census Population	2010 Census Occupied TOTAL Housing	(6) 2010 Est. Avg. Persons per Household (PPH) Occupied	(2) Total Dwelling Units (Occupied)	POP % of County	Total Percentage Change in Growth 2000-2010 Census		(7) Growth Per Year Comparison to ID DOL Econ.	2007 Previous Growth Rate		(5) Total Dwelling Units	POP % of County	Pop'n Increase	No. of New Dwelling Units to Distribute (add to 2010 values)	NEW Dwelling Units Total % of County Growth	(4) Total Population	(5) Total Dwelling Units	POP % of County	Pop'n Increase		No. of New Dwelling Units to Distribute (add to 2010 values)	NEW Dwelling Units Total % of County Growth	(4) Total POP	(5) Total Dwelling Units	POP % of County	No. of New Dwelling Units to Distribute (add to 2020 values)	POP'n Increase	TOTAL NEW Dwelling Units	NEW Dwelling Units Total % of County Growth
	Cities (Incorporated areas Only in 2010 data - ACI's are included in 2035 projections)																														
Athol	346	676	692	282	2.45	282	0.50%	2.37%	0.234%	0.000%	0.500%	699	285	0.5%	7	282	0.632%	708	289	0.4%	16	7	0.043%	0.234%	734	299	0.3%	10	42	17	0.0%
Coeur d'Alene	24,563	34,514	44,137	18,395	2.40	18,395	31.87%	27.88%	2.499%	2.400%	1.800%	48,717	20,304	31.9%	4580	20301	45.494%	56,494	23,545	31.7%	12357	5150	33.316%	2.499%	81,808	34,095	29.9%	10,550	37,671	15,700	30.0%
Dalton Gardens	1,951	2,278	2,335	883	2.64	883	1.69%	2.50%	0.247%	0.126%	0.500%	2,358	892	1.5%	23	889	1.992%	2,393	905	1.3%	58	22	0.143%	0.247%	2,484	939	0.9%	34	149	56	0.1%
Fernan (Included w/CDA)	170	186	169	72	2.35	72	0.12%	-9.14%	0.000%	0.000%	0.000%	169	72	0.1%	0	70	0.156%	169	72	0.1%	0	0	0.000%	0.000%	169	72	0.1%	0	0	0	0.0%
Harrison	226	267	203	100	2.03	100	0.15%	-23.97%	0.000%	2.146%	3.000%	203	100	0.1%	0	98	0.220%	203	100	0.1%	0	0	0.000%	0.000%	203	100	0.1%	0	0	0	0.0%
Hauser Lake	380	668	678	302	2.25	302	0.49%	1.50%	0.149%	0.010%	2.500%	682	304	0.4%	4	302	0.676%	688	307	0.4%	10	5	0.029%	0.149%	704	313	0.3%	7	26	11	0.0%
Hayden	3,744	9,159	13,294	5,212	2.55	5,212	9.60%	45.15%	3.796%	1.675%	3.000%	15,430	6,050	10.1%	2136	6047	13.551%	19,296	7,565	10.8%	6002	2353	15.223%	3.796%	33,742	13,229	12.3%	5,664	20,448	8,017	15.3%
Hayden Lake	338	494	574	256	2.24	256	0.41%	16.19%	1.512%	0.621%	1.000%	610	272	0.4%	36	270	0.604%	667	297	0.4%	93	41	0.268%	1.512%	835	373	0.3%	75	261	117	0.2%
Huetter (Incl w/Post Falls)	82	96	100	42	2.38	42	0.07%	4.17%	0.409%	1.000%	1.000%	102	43	0.1%	2	40	0.090%	104	44	0.1%	4	2	0.011%	0.409%	111	47	0.0%	3	11	5	0.0%
Post Falls	7,249	17,247	27,574	10,263	2.69	10,263	19.91%	59.88%	4.801%	3.001%	2.800%	33,263	12,380	21.8%	5689	12378	27.737%	44,071	16,403	24.7%	16497	6140	39.723%	4.801%	89,050	33,144	32.6%	16,741	61,476	22,881	43.8%
Rathdrum	2,000	4,816	6,826	2,427	2.81	2,427	4.93%	41.74%	3.549%	4.010%	3.000%	7,848	2,790	5.1%	1022	2788	6.247%	9,674	3,440	5.4%	2848	1013	6.552%	3.549%	16,324	5,804	6.0%	2,364	9,498	3,377	6.5%
Spirit Lake	790	1,376	1,945	739	2.63	739	1.40%	41.35%	3.521%	3.960%	2.000%	2,234	849	1.5%	289	846	1.896%	2,749	1,045	1.5%	804	306	1.977%	3.521%	4,620	1,755	1.7%	711	2,675	1,016	1.9%
State Line (Included w/Post Falls)	26	28	38	20	1.90	13	0.03%	35.71%	3.101%	0.000%	0.000%	43	23	0.0%	5	21	0.046%	52	27	0.0%	14	14	0.091%	3.101%	82	43	0.0%	16	44	30	0.1%
Worley	182	223	257	104	2.47	105	0.19%	15.25%	1.429%	0.400%	1.000%	272	110	0.2%	15	108	0.241%	296	120	0.2%	39	15	0.096%	1.429%	366	148	0.1%	28	109	43	0.1%
Urban Totals	42,047	72,028	98,822	39,097	2.41	39,091	71.35%	18.61%	3.25%	1.668%	2.3%	112,629	44,472	74%	13,807	44470	99.653%	137,565	54,158	77%	38,743	15,067	97.472%	3.253%	231,230	90,361	84.5%	36,203	132,408	51,270	98%
Unincorporated County (Outside of Incorporated areas)																															
Post Falls Highway District			10,844	4,135	2.62	4,139	7.64%		0.217%			10,939	4,175	7.2%	94	36	0.081%	11,082	4,230	6.2%	238	91	0.587%	0.217%	11,448	4,369	27.0%	140	604	230	0.4%
Lakes Highway District			18,704	6,902	2.71	6,902	12.74%		0.363%			18,977	7,003	12.4%	273	101	0.226%	19,395	7,157	10.9%	690	255	1.648%	0.363%	20,478	7,556	48.4%	400	1,773	654	1.3%
Eastside Highway District			5,970	2,429	2.46	2,427	4.48%	8.22%	0.128%			6,001	2,439	3.9%	31	12	0.028%	6,047	2,458	3.4%	77	31	0.202%	0.128%	6,164	2,506	14.6%	48	194	79	0.2%
Worley Highway District			4,155	1,636	2.54	1,636	3.02%		0.086%			4,170	1,642	0	14	6	0.013%	4,191	1,650	0	36	14	0.091%	0.086%	4,245	1,671	10.0%	21	90	35	0.1%
Unincorporated Totals:	27,748	36,657	41,555	15,103	2.58	15,104	28.65%	Overall % AVERAGE	0.794%	1.700%	1.000%	40,087	15,259	26.25%	412	155	0.347%	40,715	15,495	23%	1040	391	Overall % AVERAGE	0.794%	42,336	16,103	15.5%	608	2,664	999	2%
DISTRICTWIDE:																															
Post Falls Highway Dept.			54,224	21,603	2.51		39.15%					59,792	23,808	39%	5568			69,801	27,763	39%					107,107	42,436	39.2%				
Lakes Highway District			63,423	23,906	2.65		45.79%					69,935	26,346	46%	6513			81,642	30,723	46%					125,278	46,960	45.8%				
Eastside Highway Dist			12,241	5,163	2.37		8.84%					13,499	5,690	9%	1257			15,758	6,635	9%					24,180	10,142	8.8%				
Worley Highway District			8,607	3,526	2.44		6.21%					9,491	3,886	6%	884			11,080	4,531	6%					17,001	6,926	6.2%				
			138,495	54,198	2.49		100.00%					152,716	59,731	100%	14222			178,280	69,653	100%					273,567	106,464	100.0%				
Note: County will do TAZ distribution for areas outside the ACI's. Distribution may differ from highway district totals above, however total unincorporated county values should match this table.																															
Kootenai County Total	69,795	108,685	138,494	54,200	2.52	54,195	100.00%	26.84%	2.40%	2.1%	1.000%	152,716	59,731	100.0%	14,219	44,625	100.000%	178,280	69,653	100.0%	39,783	15,458	100.000%	2.400%	273,566	106,464	100.0%	36,812	135,072	52,269	100%

NOTES:
 PPH = Persons per Household
 2.30 w/11% surplus housing (2.4 - 0.11)
 2.41 Urban Average
 2.52 overall Kootenai PPH (as reported by US Census Bureau)

- (1) 2010 population based on us Census Bureau Decennial Census Data.
- (2) Dwelling Units based on PPH combined average of occupied single and multi-family units from US Census Bureau Records for 2010.
- (3) Anticipated Average Annual Growth Rates were updated in 2010, Based on Historical growth rate from 2010 US Census Bureau Data, from 2000 to 2010.
- (4) 2035 Population Projections were based on the 2010 US Census Bureau population compounded annually using Projections*, over 25 years.
- (5) 2020 & 2035 Dwelling units were calculated by dividing 2035 projected population by average (PPH) household size from US Census Bureau 2010 (Direct calculation 2010 Pop/Total # Occupied Dwelling Units)
- (6) Persons per household, calculated for cities by dividing the known Population by the known occupied housing units. (Population/Occupied Dwelling Units).
- (7) Estimate from Alivia Metts, Regional Economist, IMPLAN January 6, 2012.
 Note: Unincorporated area totals determined by using Kootenai County Structure (dwelling units) file and Highway District Boundaries in GIS.

Appendix H

Urban & Rural Federal Functional Classifications

2025 URBAN FEDERAL FUNCTIONAL CLASSIFICATION

KMPO Board Approved
December 12, 2013

FHWA Approval
Signature & Date

R. Scott Frey

November 17, 2014

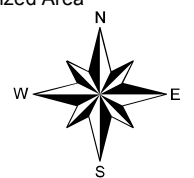
LEGEND

Classification

- Interstate
- Other Freeways or Expressways
- Other Principal Arterials
- Minor Arterial
- Major Collector
- Minor Collector
- Future Interstate
- Future Other Freeway/Expressway
- Future Other Principal Arterial
- Future Minor Arterial
- Future Major Collector
- Future Minor Collector

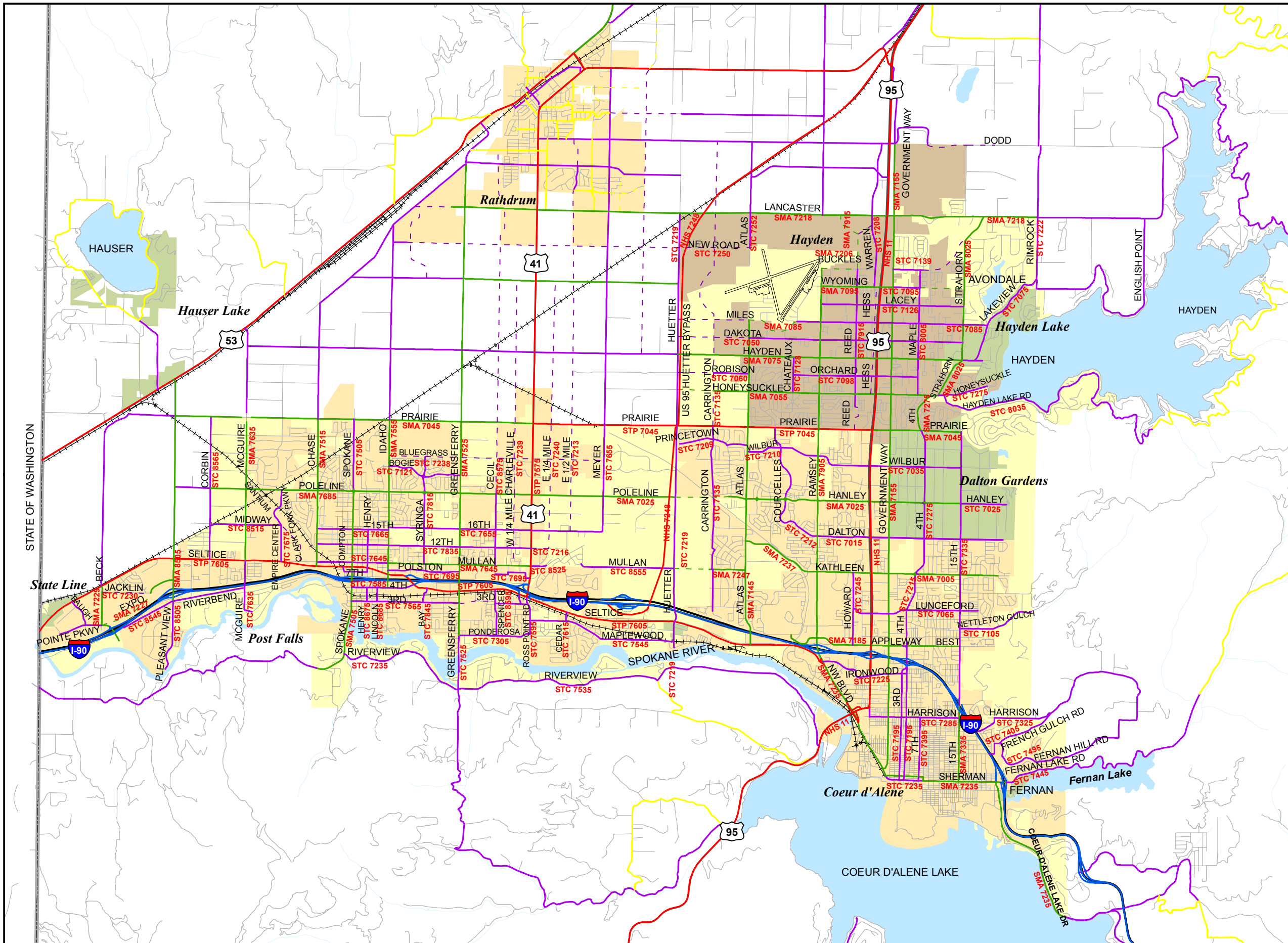
Jurisdictions & Other Features

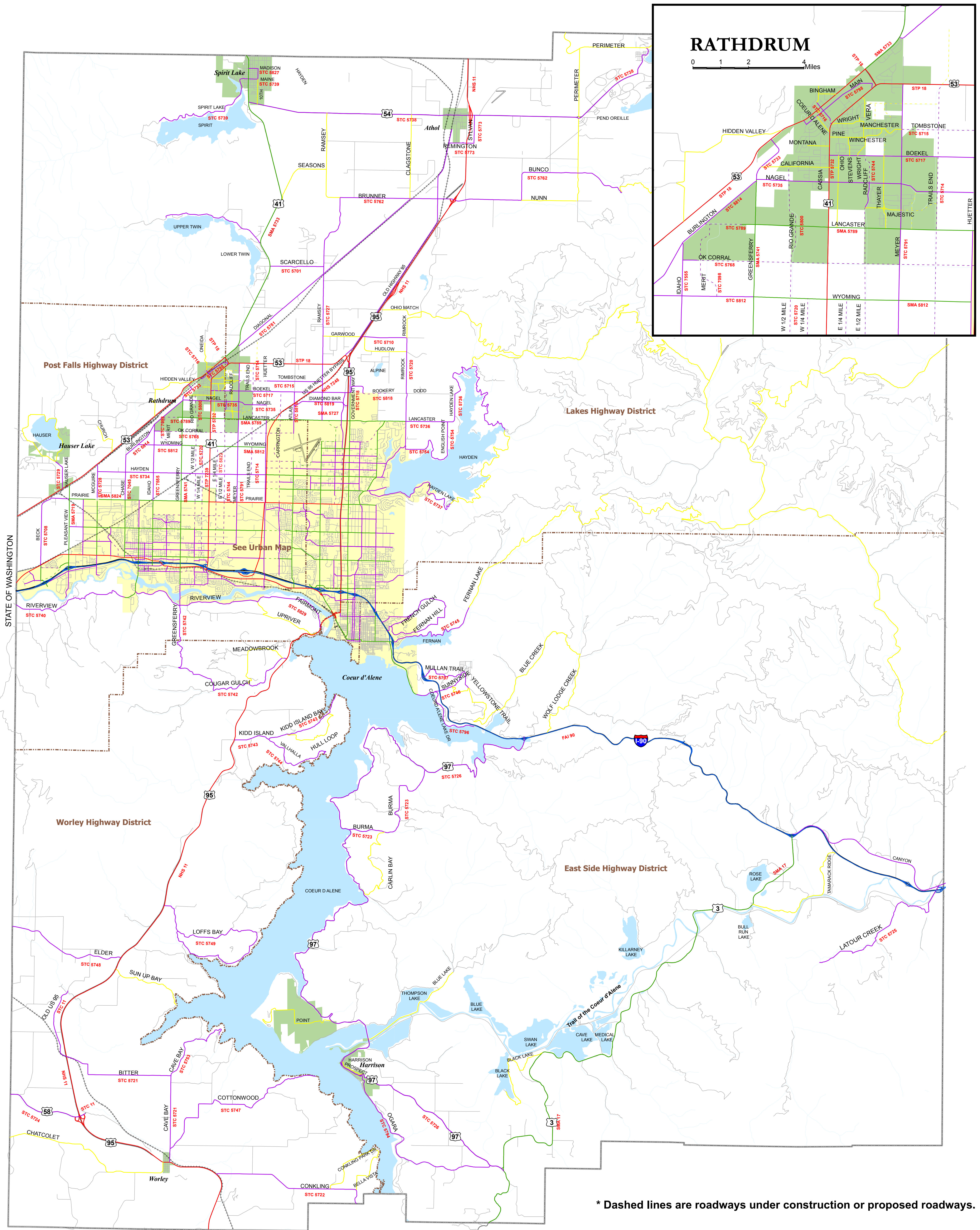
- CDA; Post Falls; Rathdrum
- Hayden
- Other Jurisdictions
- Water Features
- Urbanized Area



KOOTENAI METROPOLITAN PLANNING ORGANIZATION
250 Northwest Blvd., Suite 209, Coeur d'Alene, Idaho 83814

*Data based on best available information.
*Data for illustrative purposes only.





* Dashed lines are roadways under construction or proposed roadways.

2025 RURAL FEDERAL FUNCTIONAL CLASSIFICATION

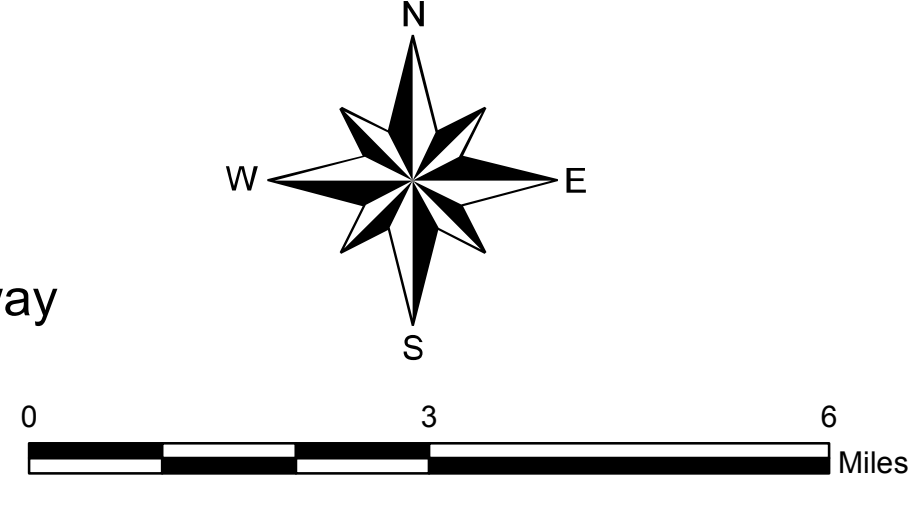
KOOTENAI COUNTY, IDAHO

Partner Agencies:
 City of Coeur d'Alene, City of Post Falls, City of Hayden,
 City of Rathdrum, Coeur d'Alene Tribe, East Side Highway District,
 Idaho Transportation Department, Kootenai County, Lakes Highway District,
 Post Falls Highway District, Worley Highway District & Small cities and towns.



LEGEND

Classification		
— Interstate	— Future Interstate	
— Other Freeways or Expressways	— Future Other Freeway/Expressway	
— Other Principal Arterials	— Future Other Principal Arterial	
— Minor Arterial	— Future Minor Arterial	
— Major Collector	— Future Major Collector	
— Minor Collector	— Future Minor Collector	
Jurisdictions & Other Features		
Water Features		
Jurisdictions		
Urbanized Area		



KMPO Board Approved
 December 12, 2013

FHWA Approval Signature & Date

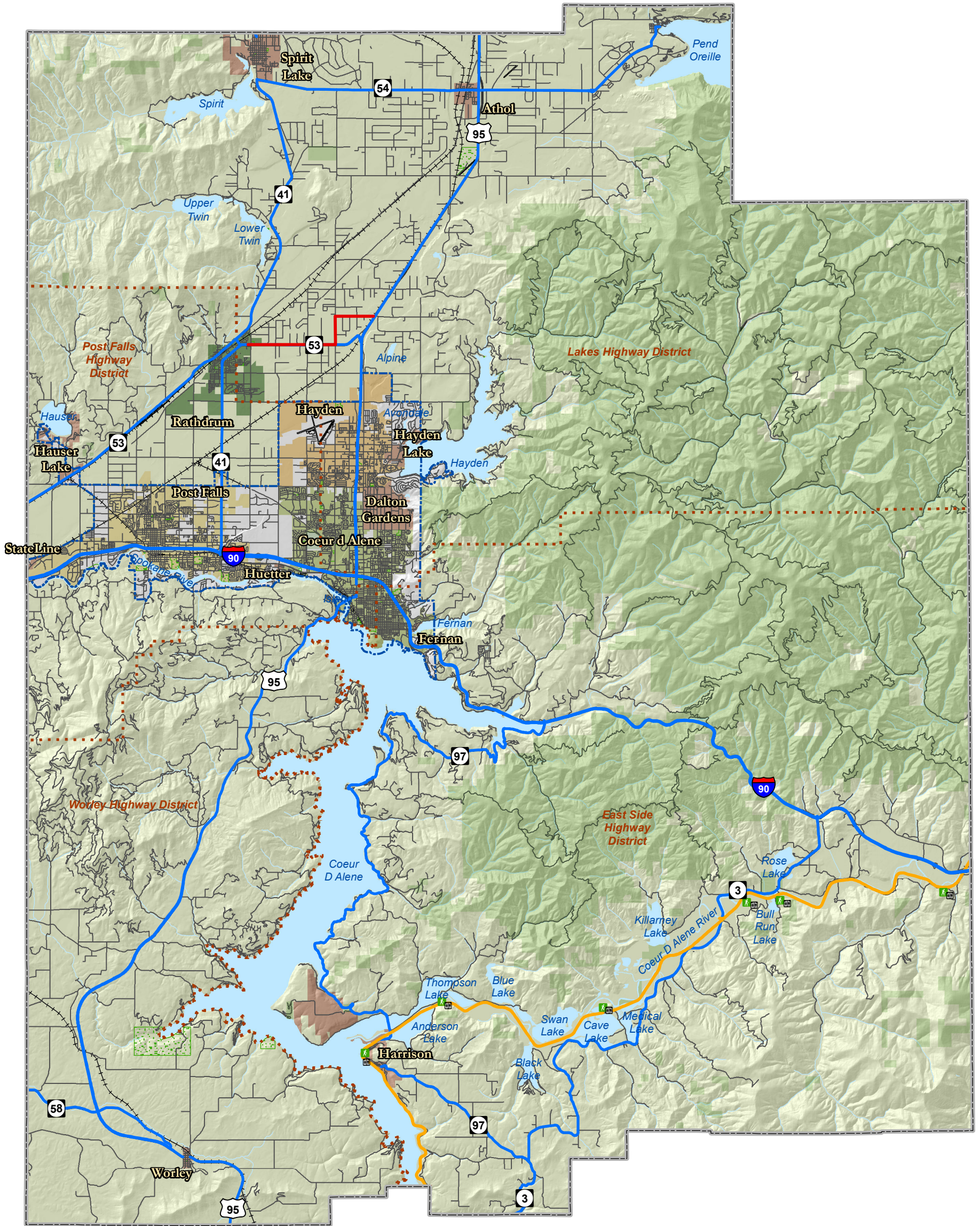
R. Scott Frey November 17, 2014

*Data based on best available information.
 *Data for illustrative purposes only.

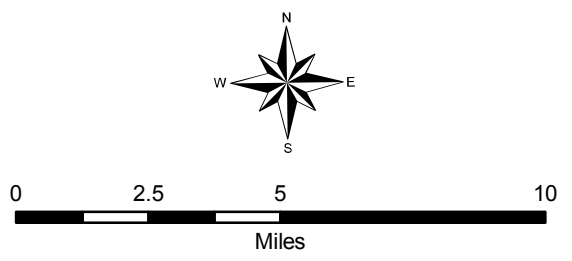
Appendix I

Inter-Model Maps

KOOTENAI METROPOLITAN TRANSPORTATION PLAN 2017-2040



NON-MOTORIZED PATHWAYS RURAL, KOOTENAI COUNTY



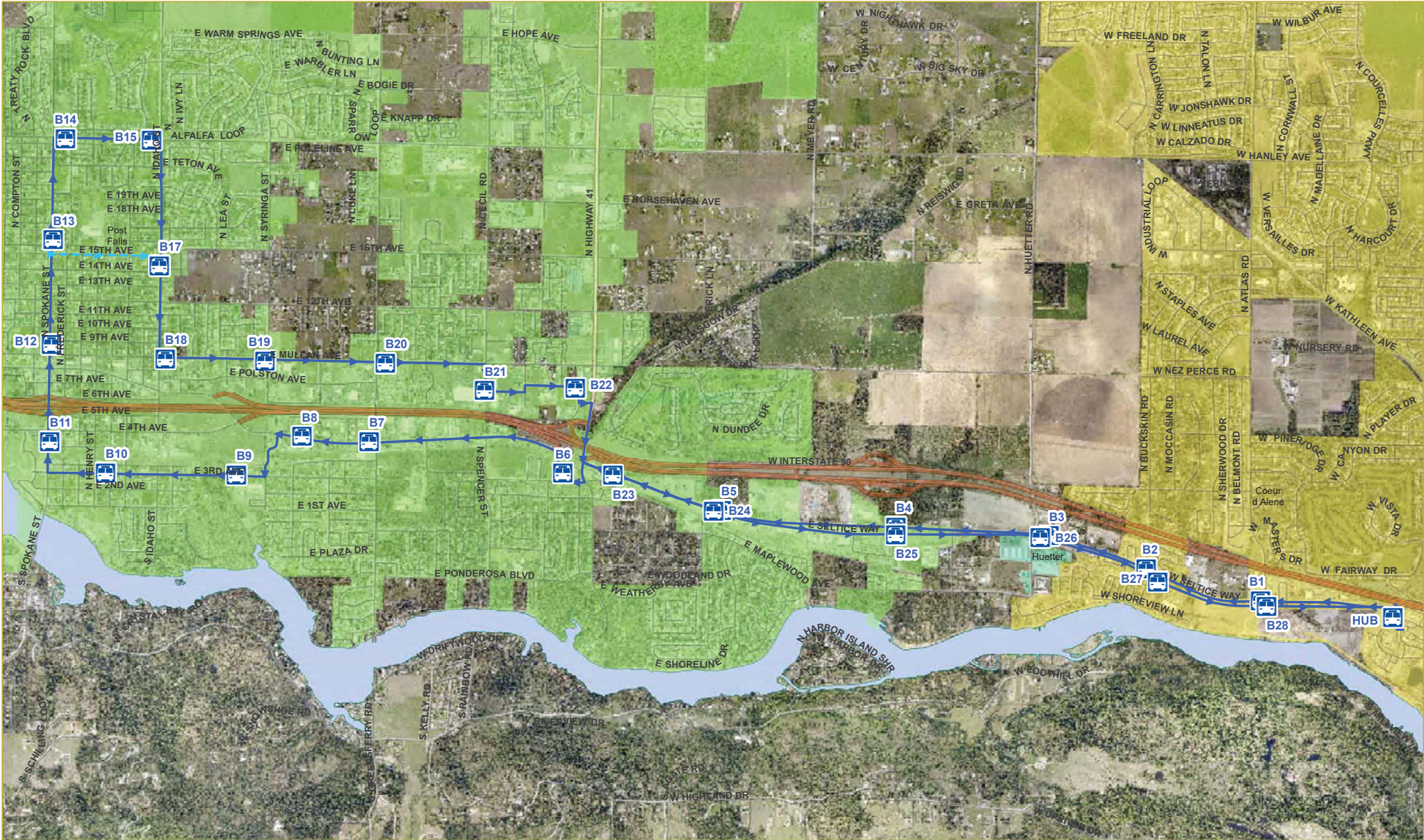
Pathways & Amenities

- Shared Use Path
- Bicycle Lane
- Shared Roadway
- Trailhead
- Drinking Fountain
- Restroom
- Parking

Physical Characteristics

- - - Highway Districts
- Interstate
- US/State Highways
- Local/Seasonal Roads
- + + + Railroad
- County Boundary
- Urban Area Boundary
- National Forests
- Water Features
- Parks

*Data based on best available information. *Data for illustrative purposes only.



Kootenai County GIS
 PO Box 9000 Prepared for: KC GMO
 451 N Government Way Prepared by: Kootenai County GIS
 Coeur d'Alene, ID 83814 Issue Date: 06-29-2016
 email: kcgis@kcgov.us Project Filename: Poss_Bus_Routes
 www.co.kootenai.id.us Plot File:
This map is to be used for reference purposes only.
 Kootenai County is not responsible for any inaccuracies contained herein.

Kootenai County
Possible Bus Routes (Urban Route B)

0 0.5 1 Miles

NewStopsCombined
 A Route
 C Route
 B Alt
 B Route
 A Alt
 C Alt

Appendix J

Post Falls Highway District Map

Appendix K

Capital Improvement Plan and CIP Project Map



**POST FALLS HIGHWAY DISTRICT
CAPITAL IMPROVEMENTS PLAN
2018-2022
(Draft)**

Anticipated Year of Construction	Project	Type of Capital Improvement	CIP Value	Potential Funding Source	Approved Funding Source	Design Year
2018	Prairie Avenue & Chase Road (roundabout)	Intersection Improvements	\$1,000,000		Strategic Initiative Grant	2017
2018	Huetter Road (Mullan to Prairie, widen shoulders & structural overlay)	Road Improvements	\$400,000		DA, HB312, PFHD	2017
2018	Hauser Lake Road (replace culvert west of Ragged Ridge Road)	Bridges/Culverts	\$30,000		PFHD	2017
2018	Winch Avenue - Church to N. Chase (ADT - 84 Length - 3464')	Bituminous Surface Treatment	\$40,000		PFHD	-
2018	Schilling Loop - Eastside, Riverview to Coyote (ADT - 195, Length - 2709')	Bituminous Surface Treatment	\$32,000		PFHD	-
2019	Riverview Drive (guardrail installation)	Safety Improvements	\$450,000		LHSIP	2017
2019	Riverview Drive at Skalen Creek (widening and guardrails)	Safety Improvements	\$1,000,000		STP Rural	2017
2019	Hauser Lake Road Culvert sizing/elevate roadway)	Road Improvements	\$150,000		HB312 - PFHD	2018
2019	Prairie Avenue (SH-41 to Greensferry, CRABS & overlay)	Road Improvements	\$400,000	HB312 - PFHD	PFHD	2019
2019	Wyoming Avenue (Meyer Rd to Huetter rebuild to gravel standards)	Road Improvements	\$100,000	PFHD		2017
2019	Schilling Loop - Coyote to Comet (ADT - 189 Length - 4179)	Bituminous Surface Treatment	\$50,000		PFHD	-
2020	Spoakne St Bridge Rehab	Bridges/Culverts	\$590,000		STP Bridge	2017
2020	Pleasant View & Prairie Avenue (roundabout)	Intersection Improvements	\$1,200,000	LHSIP	LHSIP	2018
2020	Prairie Avenue & Greensferry Road (signalization)	Intersection Improvements	\$600,000		HB312, PFHD	2019
2020	Seltice Way (Seeley St to Huetter Overlay)	Road Improvements	\$450,000	HB312, PFHD		2020
2020	S. Stateline Rd - End of oil to bottom of hill (ADT - 158 Length 4280')	Bituminous Surface Treatment	\$50,000		PFHD	-
2021	Prairie Avenue & Idaho Street	Intersection Improvements	\$990,000		LHSIP	2020
2021	Riverview Drive (Curve Realignment at St. Dominics)	Road Improvements	\$500,000		DA - PFHD	2021
2021	Riverview Drive (safety improvements)	Safety Improvements	\$450,000	HB312, PFHD		2017
2021	Millsap Loop - Holland Rd to the elk pens (ADT - 104 Length - 3400')	Bituminous Surface Treatment	\$40,000		PFHD	-
2022	Beck Road, Seltice Way to Prairie Avenue (widen shoulders & structural overlay)	Road Improvements	\$2,500,000		STP Rural	2017
2022	Millsap Loop (Big Rock Road rebuild to intersection & hill)	Road Improvements	\$50,000		PFHD	2021
2022	Millsap Loop - Elk pens to Deer Ridge (ADT - 104 Length - 5787')	Bituminous Surface Treatment	\$68,000		PFHD	-
2023	Hayden Avenue & Meyer Road (intersection improvements)	Intersection Improvements	\$1,200,000	STP, PFHD		2019
Preliminary / Not Yet Scheduled	Prairie Avenue (Five Lanes from Meyer to SH-41)	Road Improvements	\$4,200,000	STP/HB312, PFHD		
	Riverview Drive Extension (Foothills to Fairmont Loop)	Road Improvements	NR	Development, STP, HB312, PFHD		
	Upriver Drive (Realignment at Jacobs Loop)	Road Improvements	\$75,000	PFHD		
	Riverview Drive (intersection improvements at Idaho Road)	Intersection Improvements	\$500,000	PFHD		
	Pleasant View & SH-53 (grade seperated intersection)	Intersection Improvements	\$2,000,000	Tiger Grant		
	Riverview Drive & Harbor Drive (intersection improvements)	Intersection Improvements	\$150,000	PFHD		
	Huetter Road Bikeway (connect the Prairie Path to the Prairie Avenue bike lanes)	Bike/Pedestrian	\$200,000	Children Pedestrian Safety-TAP Grant		
	Seltice Way Connection to the Centennial Trail (connect the Seltice Shared Use Path to the Centennial Trail)	Bike/Pedestrian	\$50,000	Children Pedestrian Safety-TAP Grant		
	Greensferry Bridge	Bridges/Culverts	\$16,000,000			
Total CIP Infrastructure Improvements			\$35,515,000			



POST FALLS HIGHWAY DISTRICT ROAD NETWORK MAP KOOTENAI COUNTY, IDAHO 2018

LEGEND

- HIGHWAY DISTRICT BOUNDARY
- SECTION LINE
- RAILROAD TRACKS
- INTERSTATE HIGHWAY
- U.S. HIGHWAY
- STATE HIGHWAY
- PAVED ROAD
- GRAVEL ROAD
- UNMAINTAINED ROAD
- DEDICATED RIGHT-OF-WAY
- DEDICATED RIGHT-OF-WAY WITH EXISTING TRAVELWAY
- POST FALLS HIGHWAY DISTRICT OFFICE

CIP PROJECT KEY

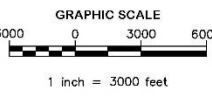
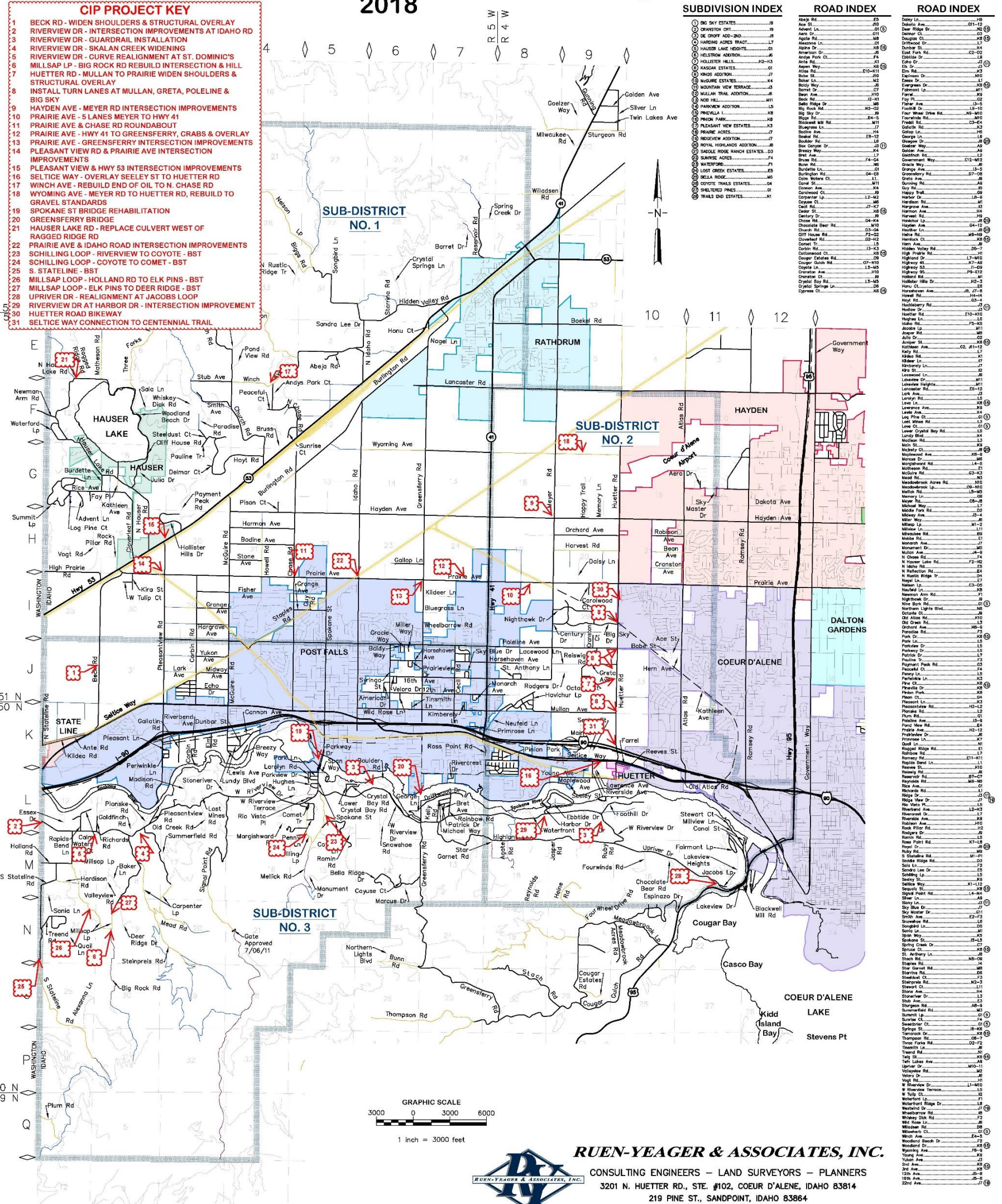
- 1 BECK RD - WIDEN SHOULDERS & STRUCTURAL OVERLAY
- 2 RIVERVIEW DR - INTERSECTION IMPROVEMENTS AT IDAHO RD
- 3 RIVERVIEW DR - GUARDRAIL INSTALLATION
- 4 RIVERVIEW DR - SKALAN CREEK WIDENING
- 5 RIVERVIEW DR - CURVE REALIGNMENT AT ST. DOMINIC'S
- 6 MILLSAP LP - BIG ROCK RD REBUILD INTERSECTION & HILL
- 7 HUETTER RD - MULLAN TO PRAIRIE WIDEN SHOULDERS & STRUCTURAL OVERLAY
- 8 INSTALL TURN LANES AT MULLAN, GRETA, POLELINE & BIG SKY
- 9 HAYDEN AVE - MEYER RD INTERSECTION IMPROVEMENTS
- 10 PRAIRIE AVE - 5 LANES MEYER TO HWY 41
- 11 PRAIRIE AVE & CHASE RD ROUNDABOUT
- 12 PRAIRIE AVE - HWY 41 TO GREENSFERRY, CRABS & OVERLAY
- 13 PRAIRIE AVE - GREENSFERRY INTERSECTION IMPROVEMENTS
- 14 PLEASANT VIEW RD & PRAIRIE AVE INTERSECTION IMPROVEMENTS
- 15 PLEASANT VIEW & HWY 53 INTERSECTION IMPROVEMENTS
- 16 SELTICE WAY - OVERLAY SEELEY ST TO HUETTER RD
- 17 WINCH AVE - REBUILD END OF OIL TO N. CHASE RD
- 18 WYOMING AVE - MEYER RD TO HUETTER RD, REBUILD TO GRAVEL STANDARDS
- 19 SPOKANE ST BRIDGE REHABILITATION
- 20 GREENSFERRY BRIDGE
- 21 HAUSER LAKE RD - REPLACE CULVERT WEST OF RAGGED RIDGE RD
- 22 PRAIRIE AVE & IDAHO ROAD INTERSECTION IMPROVEMENTS
- 23 SCHILLING LOOP - RIVERVIEW TO COYOTE - BST
- 24 SCHILLING LOOP - COYOTE TO COMET - BST
- 25 S. STATELINE - BST
- 26 MILLSAP LOOP - HOLLAND RD TO ELK PINS - BST
- 27 MILLSAP LOOP - ELK PINS TO DEER RIDGE - BST
- 28 UPRIVER DR - REALIGNMENT AT JACOBS LOOP
- 29 RIVERVIEW DR AT HARBOR DR - INTERSECTION IMPROVEMENT
- 30 HUETTER ROAD BIKEWAY
- 31 SELTICE WAY CONNECTION TO CENTENNIAL TRAIL

SUBDIVISION INDEX

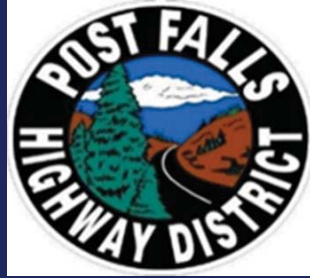
- 01 BIG SKY ESTATES.....18
- 02 GRANITE CRT.....19
- 03 DE GROFF ADD-2ND.....18
- 04 HARDING ACRES TRACT.....17
- 05 HAUSER LAKE HOUSES.....21
- 06 HELSTON ADDITION.....16
- 07 HOLISTER HILLS.....12-13
- 08 KASCAK ESTATES.....21
- 09 KING ADDITION.....17
- 10 MCGUIRE ESTATES.....14
- 11 MOUNTAIN VIEW TERRACE.....13
- 12 PINEHILL.....18
- 13 PRINCE PARK.....18
- 14 PLEASANT VIEW ESTATES.....13
- 15 PRINCE ACRES.....17
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- 17 SAGEHILL RIDGE ESTATES.....23
- 18 SUNRISE ACRES.....14
- 19 WATERFORD.....11
- 20 WEST CREEK ESTATES.....15
- 21 BELLA RIDGE.....04
- 22 COYOTE TRAILS ESTATES.....04
- 23 ENHANCED PINES.....01
- 24 CARWOOD CL.....12-13
- 25 CORPUS CR.....11
- 26 DALLAS RD.....17-18
- 27 DALLAS RD.....17-18
- 28 DALLAS RD.....17-18
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ROAD INDEX

- 01 Beck Rd.....18
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- 100 Beck Rd.....18



RUEN-YEAGER & ASSOCIATES, INC.
CONSULTING ENGINEERS - LAND SURVEYORS - PLANNERS
3201 N. HUETTER RD., STE. #102, COEUR D'ALENE, IDAHO 83814
219 PINE ST., SANDPOINT, IDAHO 83864



**Post Falls Highway District
Transportation Plan**